

**AUGLAIZE COUNTY, CLARK COUNTY,  
MERCER COUNTY, MIAMI COUNTY,  
MONTGOMERY COUNTY**

ALL LOCATION MAPS  
AND DATA ARE LOCATED  
ON SHEETS 2-3

### PROJECT DESCRIPTION

DISTRICT WIDE PROJECT TO PERFORM STRUCTURAL STEEL REPAIRS ON VARIOUS BRIDGES THROUGHOUT THE DISTRICT, TO INCLUDE REFURBISH, RESETTling, AND REPLACING BEARINGS, REPLACE STRIP SEAL GLANDS, MINOR BRIDGE DECK PATCHING, AND MINOR SLOPE PROTECTION.

### EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: N/A #  
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A #  
NOTICE OF INTENT EARTH DISTURBED AREA: N/A #

• **MAINTENANCE PROJECT**

**LIMITED ACCESS**

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

## 2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

## CONFORMED SET

FEDERAL PROJECT NO. E171(114)

PID NO.  
**105417**

CONSTRUCTION PROJECT NO.

## IOY

**D07-BH-FY20(B)**

$$\frac{1}{67}$$

**INDEX OF SHEETS:**

TITLE SHEET	1
LOCATION / DESIGN DESIGNATION	2,3
GENERAL NOTES	4,5
MAINTENANCE OF TRAFFIC	6-36
GENERAL SUMMARY	37,38
ESTIMATED QUANTITIES	39
STRUCTURES 20' SPAN AND OVER:	
AUG-33-1507L	40
AUG-66-1273	41
CLA-4-0611R	42
CLA-54-0561	43
CLA-70-0173	44
CLA-70-1446	45
CLA-70-2631	46
CLA-72-0863	47
CLA-334-0086	48
CLA-334-0099	49
MER-29-1429 L&R	50
MIA-55-1183	51,52
MOT-4-2050L	53
MOT-35-1395 L&R	54
MOT-35-1629R	55
MOT-48-1214	56
MOT-49-1091L	57

STRUCTURES 20' SPAN AND OVER (Con't.):

MOT-75-0373L	58
MOT-75-1446	59
MOT-75-1503	60,61
MOT-235-0022L	62,63
MOT-675-0063R	64
MOT-675-0129	65
MOT-675-0233	66
MOT-675-0647	67

**DESIGN DESIGNATION: NONE REQUIRED**

DESIGN EXCEPTIONS: NONE REQUIRED

## UNDERGROUND UTILITIES

**Contact Two Working Days  
Before You Dig**



**OHIO811, 8-1-1, or 1-800-362-2764**  
(Non-members must be called directly)

PLAN PREPARED BY:  
ODOT DISTRICT 7 - ENGINEERING  
1001 ST. MARYS AVE.  
SIDNEY, OHIO 45365

ENGINEERS SEAL:



SIGNED: \_\_\_\_\_  
DATE: 11-15-19

## STANDARD CONSTRUCTION DRAWINGS

		MT-95.30	7-19-15
		MT-95.31	7-19-15
		MT-95.32	4-19-15
		MT-95.45	4-19-15
		MT-95.50	7-21-17
		MT-97.10	4-19-15
	GSD-I-96	7-19-02	
		MT-98.10	1-20-17
		MT-98.20	4-19-15
		MT-98.22	1-20-17
		MT-98.28	1-20-17
		MT-102.20	4-19-15
		MT-105.10	7-19-15
		MT-110.10	7-19-15

**SUPPLEMENTAL SPECIFICATIONS**

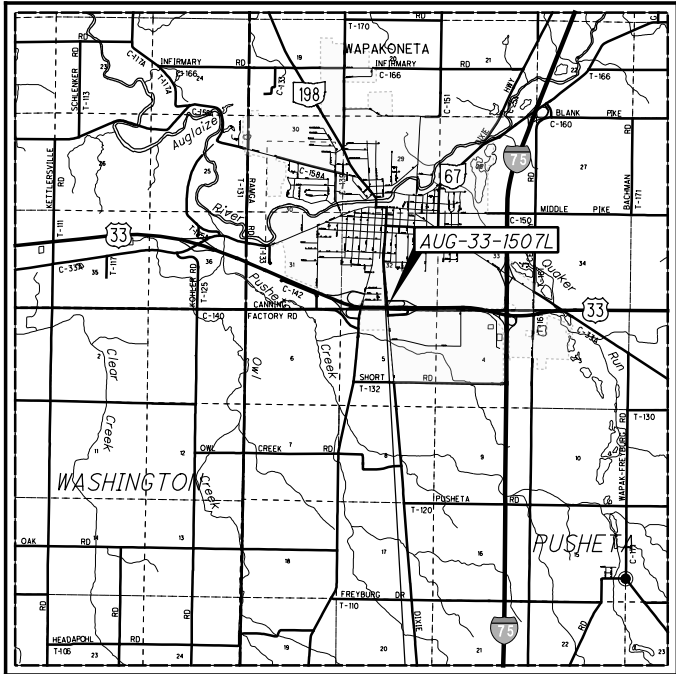
800	10-18-19
821	4-20-12
832	10-19-18
921	4-20-12

### SPECIAL PROVISIONS


APPROVED Wendy Chakelley PE, PS  
DATE 11-14-19 DISTRICT DEPUTY DIRECTOR

APPROVED [Signature]  
DATE 11/26/19 DIRECTOR, DEPARTMENT OF  
TRANSPORTATION

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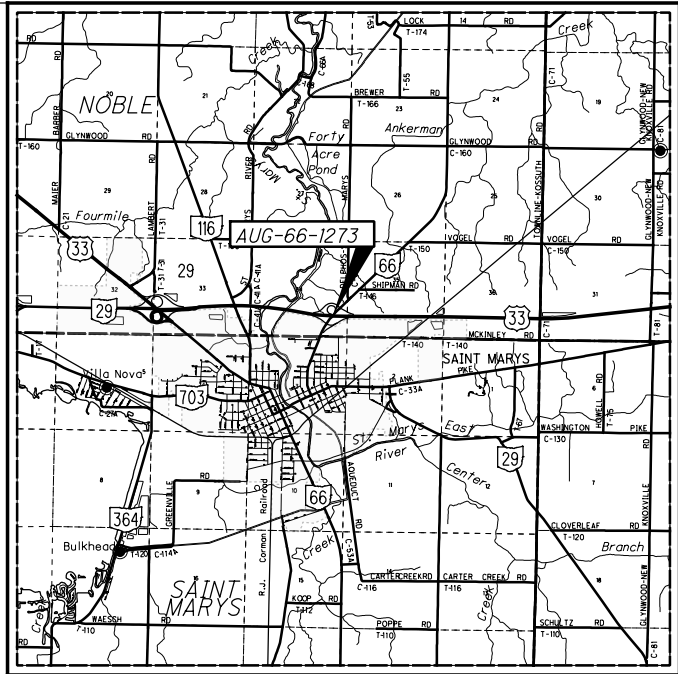
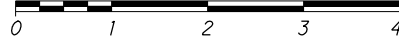


LOCATION MAP

AUG-33-1507L (SFN: 0600385)

LATITUDE: N40°33'20.02" LONGITUDE: W84°11'37.67"

SCALE IN MILES

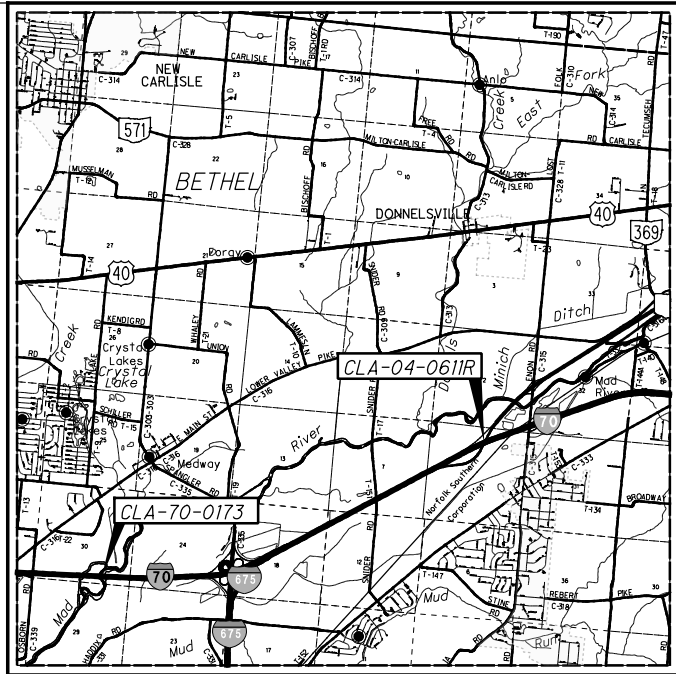
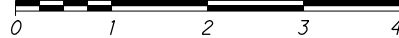


LOCATION MAP

AUG-66-1273 (SFN: 0601144)

LATITUDE: N40°33'25" LONGITUDE: W84°22'48.89"

SCALE IN MILES



LOCATION MAP

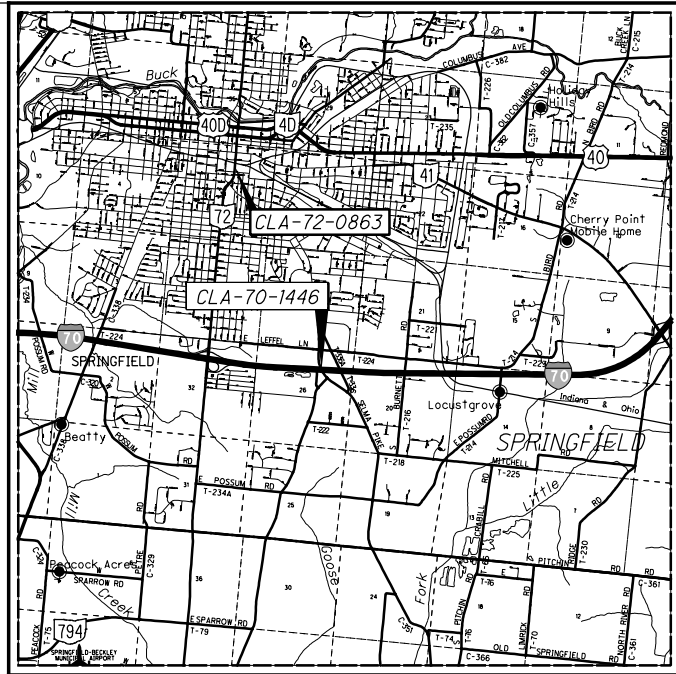
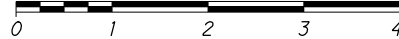
CLA-04-0611R (SFN: 1200011)

LATITUDE: N39°53'08.40" LONGITUDE: W83°56'40.49"

CLA-70-0173 (SFN: 1203630)

LATITUDE: N39°51'52.51" LONGITUDE: W84°01'08.86"

SCALE IN MILES



LOCATION MAP

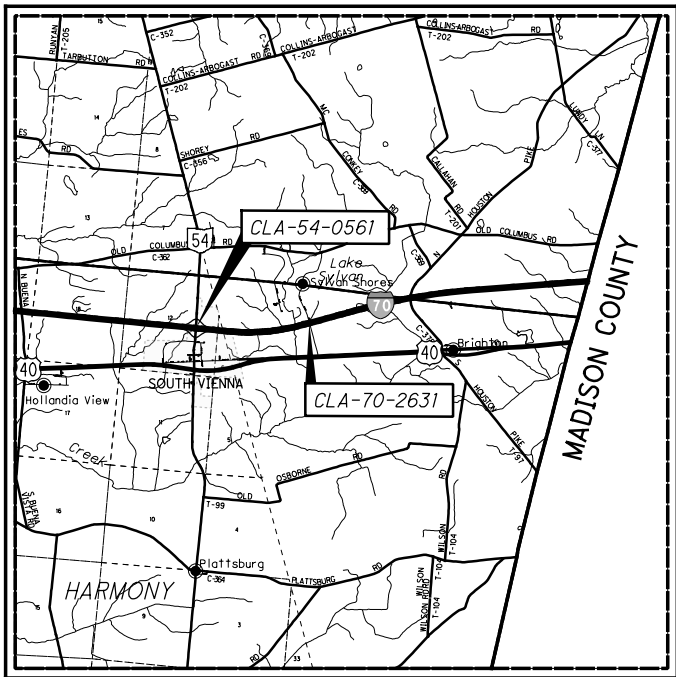
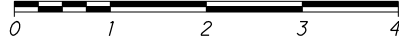
CLA-70-1446 (SFN: 1204440)

LATITUDE: N39°53'25.81" LONGITUDE: W83°47'22.20"

CLA-72-0863 (SFN: 1205463)

LATITUDE: N39°55'16.13" LONGITUDE: W83°48'25.48"

SCALE IN MILES



LOCATION MAP

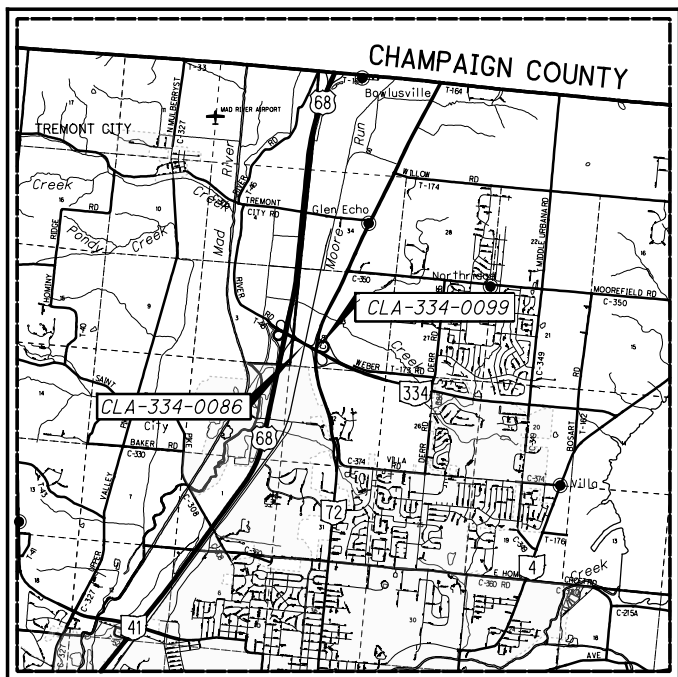
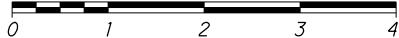
CLA-54-0561 (SFN: 1202197)

LATITUDE: N39°55'58.12" LONGITUDE: W83°36'43.02"

CLA-70-2631 (SFN: 1205250)

LATITUDE: N39°56'01.74" LONGITUDE: W83°35'22.94"

SCALE IN MILES



LOCATION MAP

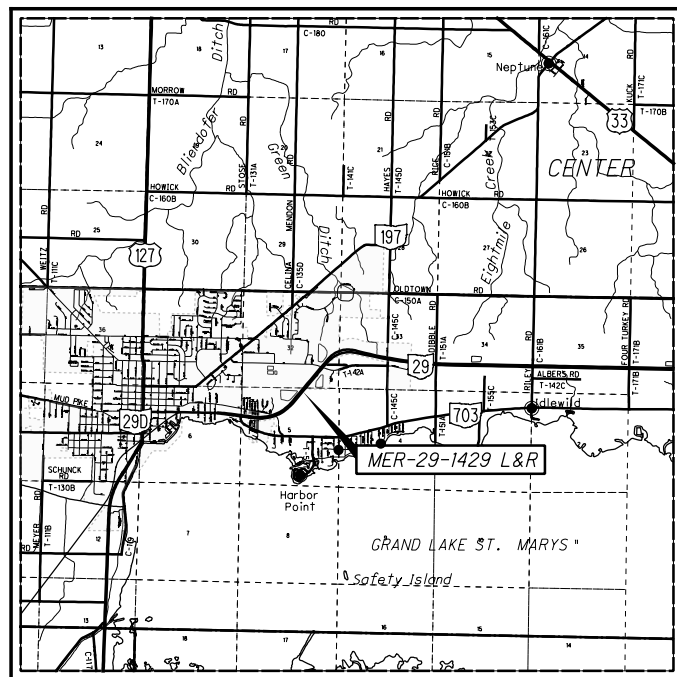
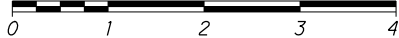
CLA-334-0086 (SFN: 1205641)

LATITUDE: N39°59'09.39" LONGITUDE: W83°48'32.27"

CLA-334-0099 (SFN: 1205617)

LATITUDE: N39°59'06.88" LONGITUDE: W83°48'26.08"

SCALE IN MILES



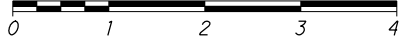
MER-29-1429L (SFN: 5400341)

LATITUDE: N40°33'11.26" LONGITUDE: W84°32'21.05"

MER-29-1429R (SFN: 5400325)

LATITUDE: N40°33'11.19" LONGITUDE: W84°32'19.66"

SCALE IN MILES



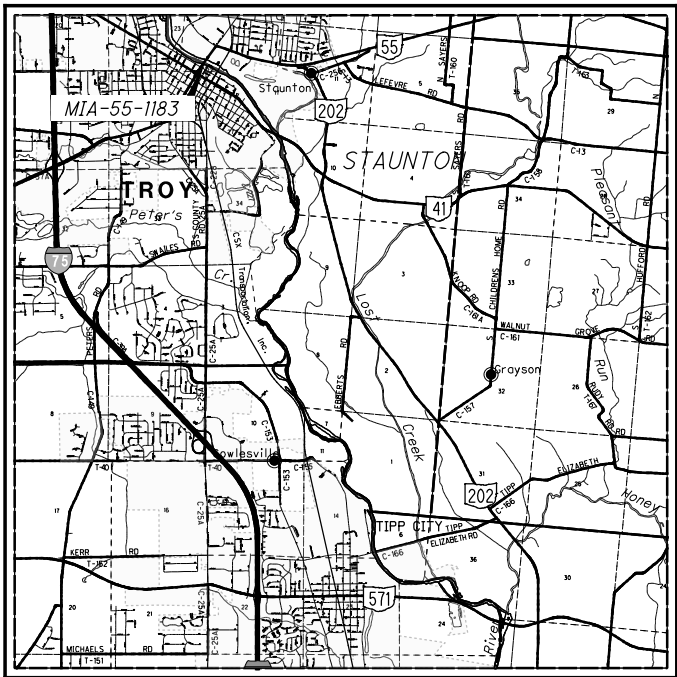
CALCULATED  
REB  
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DHG

LOCATION MAP

D07 - BH - FY 20B



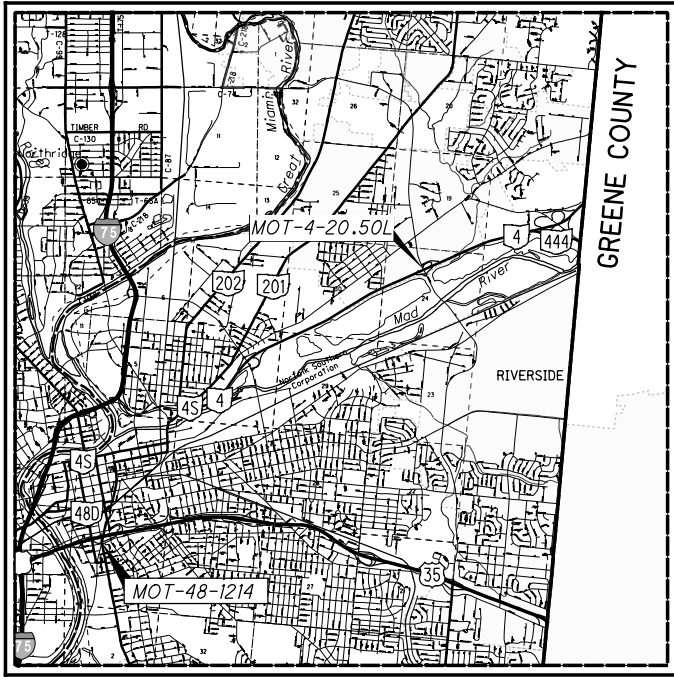
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LOCATION MAP

MIA-55-1183 (SFN: 5501504)

LATITUDE: N40°02'29.78" LONGITUDE: W84°12'06.05"



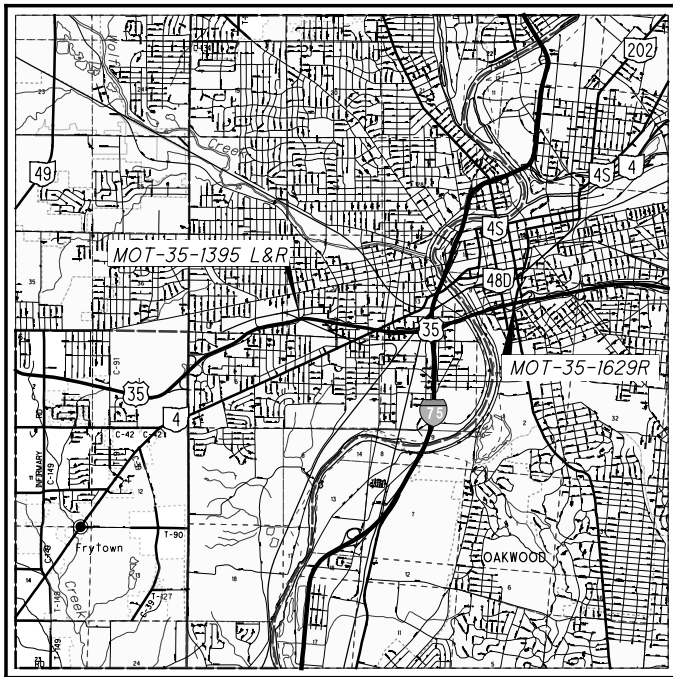
LOCATION MAP

MOT-4-2050L (SFN: 5700906)

LATITUDE: N39°47'34.49" LONGITUDE: W84°07'39.92"

MOT-48-1214 (SFN: 5703646)

LATITUDE: N39°45'03.85" LONGITUDE: W84°11'22.26"



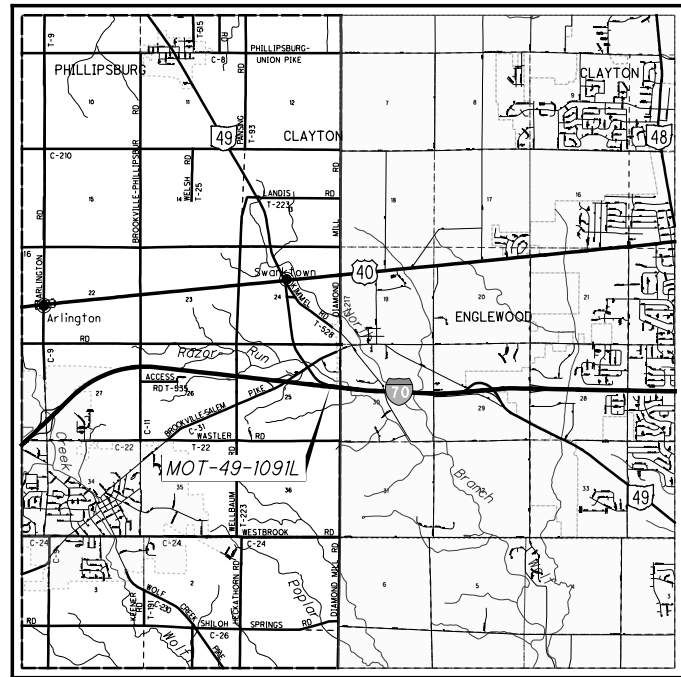
LOCATION MAP

MOT-35-1395 L&R (SFN: 5701600L/5701619R)

LATITUDE: N39°44'55.97" LONGITUDE: W84°13'53.73"

MOT-35-1629R (SFN: 5702224)

LATITUDE: N39°45'05.80" LONGITUDE: W84°11'22.28"



LOCATION MAP

MOT-49-1091L (SFN: 5704502)

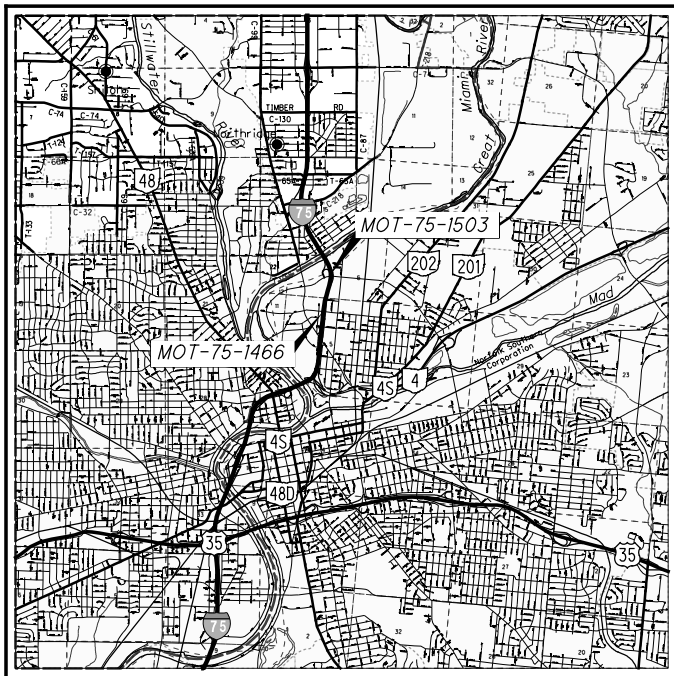
LATITUDE: N39°51'17.74" LONGITUDE: W84°22'19.54"



LOCATION MAP

MOT-75-0373L (SFN: 5706483)

LATITUDE: N39°38'22.96" LONGITUDE: W84°13'55.81"



LOCATION MAP

MOT-75-1466 (SFN: 5708613)

LATITUDE: N39°46'58.72" LONGITUDE: W84°11'06.52"

MOT-75-1503 (SFN: 5708648)

LATITUDE: N39°47'18.14" LONGITUDE: W84°11'01.57"



LOCATION MAP

MOT-675-0063R (SFN: 5710804)

LATITUDE: N39°37'19.88" LONGITUDE: W84°13'35.83"

MOT-675-0129 (SFN: 5710979)

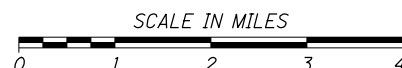
LATITUDE: N39°37'38.93" LONGITUDE: W84°12'59.55"

MOT-675-0233 (SFN: 5711126)

LATITUDE: N39°38'02.83" LONGITUDE: W84°11'55.86"

MOT-675-0647 (SFN: 5711436)

LATITUDE: N39°39'13.76" LONGITUDE: W84°07'38.82"



CALCULATED  
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DHG

LOCATION MAP

D07-BH-FY20B

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UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN IN THESE PLANS. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREAS.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK, BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFFERED TO CMS SECTIONS 102.05 AND 105.02.

EXISTING BRIDGE PLANS

EXISTING PLANS ENTITLED AUG-33-11.50, AUG-29-1.04, CLA-70-5.62, CLA-70-23.41, CLA-70-13.98, CLA-70-25.14, CLA-72-8.35, CLA-334-12.39, MIA-55-11.53, MOT-35-11.33 MOT-35-14.39, MOT-49-12.45, MOT-75-0.00, MOT-75-0.49, MOT-75-15.88, MOT-675-0.00, MOT-675-6.00, MAY BE INSPECTED IN THE ODOT DISTRICT 7 OFFICE IN SIDNEY, OHIO DURING NORMAL BUSINESS HOURS.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

COOPERATION WITH RAILROAD

THE CONTRACTOR SHALL COORDINATE ALL WORK ON OR ADJACENT TO THE RAILROADS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL CONTACT THE FOLLOWING RAILROAD COMPANY, AT LEAST 30 DAYS IN ADVANCE, IN ORDER TO COORDINATE THE NECESSARY WORK. UNDER NO CIRCUMSTANCES SHALL THERE BE ANY WORK WITHIN THE RAILROAD RIGHT OF WAY WITHOUT THE PROPER AUTHORIZATION AND/OR FLAG PROTECTION FROM THE RAILROAD.

NO INSTREAM WORK PERMITTED:

PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL MARK THE ORDINARY HIGH WATER MARK AT CLA-70-0173 AND MIA-55-1183 THROUGH CONSTRUCTION FENCING OR STAKING AND ALERT ON-SITE STAFF TO THE LOCATION OF THE ORDINARY HIGH WATER MARK. THE CONTRACTOR SHALL NOT PLACE TEMPORARY FILL OR PERMANENT FILL WITHIN ANY WETLAND OR BELOW THE ORDINARY HIGH WATER MARK OF ANY WATERWAY, INCLUDING SCAFFOLDING OR BRACING. THE CONTRACTOR SHALL NOT PLACE EQUIPMENT BELOW ORDINARY HIGH WATER MARK. IF DEBRIS ENTERS THE WATERWAY DURING CONSTRUCTION, THE CONTRACTOR SHALL REMOVE THE DEBRIS IMMEDIATELY, UTILIZING EQUIPMENT STAGED ABOVE THE ORDINARY HIGH WATER MARK.

PROTECTION OF THE MULTI-USE TRAIL AT MIA-55-1183:

THE CONTRACTOR MUST MAINTAIN ACCESS TO MULTI-USE TRAIL THROUGHOUT CONSTRUCTION. THE CONTRACTOR MAY NOT STAGE EQUIPMENT OR MATERIALS ON THE TRAIL.

PROTECTION OF WATER TRAILS AT CLA-70-0173 AND MIA-55-1183:

THE GREAT MIAMI RIVER AND THE MAD RIVER ARE DESIGNATED WATER TRAILS. THE CONTRACTOR SHALL MAINTAIN UNDER BRIDGE BOAT ACCESS TO THE WATER TRAILS THROUGHOUT CONSTRUCTION.

PROTECTION OF EASTWOOD METROPARK AT MOT-4-2050L:

THE CONTRACTOR MUST MAINTAIN ACCESS TO EASTWOOD METROPARK THROUGHOUT CONSTRUCTION. THE CONTRACTOR MAY NOT STAGE EQUIPMENT OR MATERIALS WITHIN EASTWOOD METROPARK.

PROTECTION OF CYCLISTS AT AUG-33-1507L AND MOT-675-0129:

STATE ROUTE 67 AT AUG-33-1507L AND LYONS ROAD AT MOT-675-0129 ARE DESIGNATED ON-ROAD BIKE ROUTES. CONSTRUCTION PERSONNEL SHALL BE ALERT TO CYCLISTS AND ACCOMMODATE CYCLISTS SAFE TRAVEL THROUGH THE CONSTRUCTION ZONE DURING BRIDGE WORK ACTIVITIES AT THESE LOCATION.

PROTECTION OF SIMON KENTON TRAIL AT CLA-334-0086:

THE CONTRACTOR MUST MAINTAIN ACCESS TO SIMON KENTON TRAIL THROUGHOUT CONSTRUCTION. THE CONTRACTOR MAY NOT STAGE EQUIPMENT OR MATERIALS ON THE TRAIL.

INDIANA BAT AND NORTHERN LONG-EARED BAT PROTECTION AT CLA-70-0173 AND MIA-55-1183:

PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR MUST INSPECT THE BRIDGE WORK AREA FOR EVIDENCE OF BATS. WRITTEN CONFIRMATION OF THE INSPECTION, INCLUDING A STATEMENT WHETHER EVIDENCE OF BATS WAS FOUND, MUST BE PROVIDED TO THE ODOT CONSTRUCTION ENGINEER. IF BATS ARE PRESENT WITHIN THE BRIDGE WORK AREAS, CONSTRUCTION MAY NOT OCCUR FROM APRIL 1 TO SEPTEMBER 30; ALL ACCESS MUST OCCUR FROM OCTOBER 1 TO MARCH 31.

MIGRATORY BIRD PROTECTION AT CLA-70-0173 AND MIA-55-1183:

PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR MUST INSPECT THE BRIDGE FOR EVIDENCE OF ACTIVE BIRD NESTS. WRITTEN CONFIRMATION OF THE INSPECTION, INCLUDING A STATEMENT WHETHER ACTIVE NEST WERE FOUND, MUST BE PROVIDED TO THE ODOT CONSTRUCTION ENGINEER. IF AN ACTIVE NEST CONTAINING AN EGG OR CHICK IS PRESENT, IMPACTS TO THE NEST MUST BE AVOIDED UNTIL ALL DEVELOPING BIRDS ARE ABLE TO INDEPENDENTLY FLY FROM THE NEST. NESTS THAT DO NOT CONTAIN AND EGG OR CHICK ARE CONSIDERED INACTIVE AND MAY BE REMOVED TO DISCOURAGE BIRDS FROM NESTING AND CONSTRUCTION ACTIVITIES MAY PROCEED. NESTING BIRDS MAY BE AVOIDED BY UNDERTAKING THE WORK FROM OCTOBER 1 TO MARCH 1. IF AN ACTIVE NEST CANNOT BE AVOIDED, THE CONTRACTOR MUST OBTAIN A DEPREDAION PERMIT FROM THE USFWS PRIOR TO DESTORYING ANY ACTIVE NEST. INFORMATION ON OBTAINING A DEPREDAION PERMIT MAY BE OBTAINED BY CONTACTING THE REGION 3 MIGRATORY BIRD REGIONAL PERMIT OFFICE AT 5600 AMERICAN BLVD. WEST, SUITE 990, BLOOMINGTON, MN 55437-1458; PHONE 612-713-5436. IF OPERATING UNDER A PERMIT, DOCUMENTATION MUST BE PROVIDED TO THE ODOT CONSTRUCTION ENGINEER.

CONTRACTOR'S COORDINATION NOTE

THE CONTRACTOR IS HEREBY GIVEN NOTICE THAT PROJECT MOT-741-3.62, PID 90289 MAY FALL WITHIN THE LIMITS OF THIS PLAN SET. THE START DATE FOR THIS OTHER PROJECT IS ROUGHLY ONE YEAR IN ADVANCE OF THE START DATE FOR THIS PLAN SET, HOWEVER IT MAY STILL BE IN PROGRESS WITH FINISHING WORK. THEREFORE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH THE OTHER PROJECT TO MAINTAIN TRAFFIC. ANY DISPUTES THAT ARE NOT SETTLED OR AGREED UPON, SHALL BE SETTLE BY THE PROJECTS ENGINEERS.

ITEM 202 - FENCE REMOVED FOR REUSE, AS PER PLAN.

THE QUANTITIES SHOWN ARE INCLUDED FOR GAINING ACCESS UNDER THE STRUCTURE. THE CONTRACTOR SHALL TAKE CARE IN REMOVING THE FENCE AND POSTS TO PRESERVE THEM FOR REUSE. CLEARING AND GRUBBING SHALL BE PERFORMED TO ASSURE THE FENCE CAN BE PROPERLY REALIGNED. ALL WORK, LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE PER FEET, FOR ITEM 202, FENCE REMOVED, AS PER PLAN.

APPLIES TO STR. CLA-70-0173  
SEE ESTIMATED QUANTITIES SHEET 39

ITEM 607 - FENCE REBUILT, TYPE (CLT)

CAREFULLY RECONDITION AND RE-ERECT FENCE AND COMPONENT PARTS AS DETAILED ON THE PLANS. TAKE CARE NOT TO DAMAGE THE FENCE OR COMPONENT PARTS. ANY NEW PARTS WHICH ARE NEEDED, AS DETERMINED BY THE ENGINEER, WILL BE SUPPLIED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE STATE.

THE AMOUNT OF REBUILT FENCE TO BE PAID FOR WILL BE THE NUMBER OF FEET REBUILT, COMPLETE IN PLACE, AND MEASURED AS PROVIDED FOR IN 607.09.

PAYMENT FOR THE ABOVE WILL BE PAID FOR AT THE CONTRACT PRICE PER FOOT FOR ITEM 607, FENCE REBUILT, TYPE CLT.

APPLIES TO STR. CLA-70-0173  
SEE ESTIMATED QUANTITIES SHEET 39



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ITEM 516 – RESET BEARING, AS PER PLAN

THIS ITEM SHALL INCLUDE ALL WORK NECESSARY TO PROPERLY ALIGN ROTATED AND /OR FLOATING EXISTING BRIDGE BEARINGS PER 516.07, INCLUDING REMOVAL OF UPPER BEARING PLATE WELDS TO THE EXISTING BEAM. REMOVAL OF THE ENTIRE BEARING WHERE NECESSARY TO PERFORM WORK, PROPER STORAGE OF THE BEARINGS UNTIL REINSTALLATION, REPLACEMENT OF ANY DAMAGED SHEET LEAD WITH PREFORMED BEARING PADS (711.21), INSTALLATION OF ANY NECESSARY STEEL SHIMS OF THE SAME SIZE AS THE BEARING TO PROVIDE A SNUG FIT, AND REPAIR OF DAMAGED PAINT ACCORDING TO ITEM 514.22. ALL WORK SHALL BE TO THE SATISFACTION OF THE ENGINEER. PAYMENT FOR ALL OF THE ABOVE DESCRIBED LABOR AND MATERIALS WILL BE MADE AT THE CONTRACT BID FOR ITEM 516-RESET BEARING, AS PER PLAN.

ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPER-STRUCTURE, AS PER PLAN

THIS WORK CONSISTS OF RAISING OR RE-POSITIONING EXISTING STRUCTURES TO THE DIMENSIONS AND REQUIREMENTS DEFINED IN THE PROJECT PLANS.

SUBMIT CONSTRUCTION PLANS IN ACCORDANCE WITH CMS 501.05.

IF, DURING THE JACKING OPERATIONS, CRACKING OF THE CONCRETE SUPERSTRUCTURE, SEPARATION OF THE CONCRETE DECK FROM THE STEEL STRINGERS, OR OTHER DAMAGE TO THE STRUCTURE IS VISUALLY OBSERVED, IMMEDIATELY CEASE THE JACKING OPERATION AND INSTALL SUPPORTS TO THE SATISFACTION OF THE ENGINEER. ANALYZE THE DAMAGE AND SUBMIT A METHOD OF CORRECTION TO THE ENGINEER FOR APPROVAL. EPOXY INJECT ALL BEAMS THAT SEPARATE FROM THE DECK FOR THE DISTANCE OF THE SEPARATION IN ACCORDANCE WITH CMS 512.07. THE DEPARTMENT WILL NOT PAY FOR THE COST OF THIS EPOXY INJECTION OR OTHER REQUIRED REPAIRS. THE BRIDGE BEARINGS SHALL BE FULLY SEATED AT ALL CONTACT AREAS. IF FULL SEATING IS NOT ATTAINED, SUBMIT A REPAIR PLAN TO THE ENGINEER. THE DEPARTMENT WILL NOT PAY FOR THE REPAIR COSTS TO ENSURE FULL SEATING ON BEARINGS.

THE DEPARTMENT WILL MEASURE THIS WORK ON A LUMP SUM BASIS.

THE DEPARTMENT WILL PAY FOR THE ACCEPTED QUANTITIES AT THE CONTRACT PRICE BID PER LUMP SUM FOR ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN.

ITEM 202 – PORTION OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS LISTED IN THE C&MS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING THE EXISTING ELASTOMERIC STRIP SEAL GLAND, CARE SHALL BE TAKEN THAT THE REMOVAL IS DONE WITHOUT DOING ANY DAMAGE TO THE EXISTING ANCHOR OR RETAINER CLIP. ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK SHALL BE PAID UNDER ITEM 202, PORTION OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN.

ITEM 516 – REFURBISHING BEARING DEVICE, AS PER PLAN

ITEM 516 – REFURBISHING BEARING DEVICES, AS PER PLAN: THIS ITEM SHALL INCLUDE ALL WORK NECESSARY TO PROPERLY ALIGN BRIDGE BEARINGS AS WELL AS THEIR CLEANING AND PAINTING. INCLUDED SHALL BE THE DISASSEMBLY OF THE BEARINGS, HAND TOOL CLEANING (GRINDING IF NECESSARY), PAINTING ACCORDING TO ITEM 514, REPLACEMENT OF ANY DAMAGED SHEET LEAD WITH PREFORMED BEARING PADS (711.21), INSTALLATION OF ANY NECESSARY STEEL SHIMS OF THE SAME SIZE AS THE BEARINGS TO PROVIDE A SNUG FIT, REALIGNMENT OF THE UPPER BEARING PLATE BY REMOVING EXISTING WELDS AND REWELDING SO THAT THE BEARINGS ARE VERTICALLY ALIGNED AT 60 DEGREES F, LUBRICATING SLIDING SURFACES, AND REASSEMBLY OF THE BEARINGS. ASSURE ALL BEARINGS ARE SHIMMED ADEQUATELY AND THAT NO BEAMS AND/OR BEARING DEVICES ARE “FLOATING”. AT NO ADDITIONAL COST TO THE STATE, THE CONTRACTOR MAY INSTALL NEW BEARINGS OF THE SAME TYPE AS THE EXISTING IN PLACE OF REFURBISHING THE BEARINGS. ALL WORK SHALL BE TO THE SATISFACTION OF THE ENGINEER. PAYMENT FOR ALL OF THE ABOVE DESCRIBED LABOR AND MATERIALS WILL BE MADE AT THE CONTRACT PRICE BID PER EACH FOR ITEM 516 – REFURBISH BEARING DEVICES, AS PER PLAN.

ITEM 516 – ELASTOMERIC STRIP SEAL WITHOUT STEEL EXTRUSIONS, AS PER PLAN

THE EXISTING STRIP SEAL SHALL BE REPLACED WITH ONE CONTINUOUS SEAL AS INDICATED IN THE PLANS. THE INSTALLED SEAL SHALL BE CAPABLE OF ACCOMMODATING 4” OF MOVEMENT. THE CONTRACTOR SHALL ENSURE THAT THE RETAINER CLIPS ARE FREE OF DEBRIS BEFORE INSTALLING THE NEW SEAL. INSTALL SEAL USING LUBRICANT PER THE MANUFACTURES SPECIFICATION.

ITEM 613 – LOW STRENGTH MORTAR BACKFILL, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING THE PROPOSED LOW STRENGTH MORTAR BACKFILL UNDER THE EXISTING ABUTMENT AND INSURING THE LSM PROVIDES A UNIFORM BEARING BETWEEN THE ABUTMENT AND AGGREGATE. THE CONTRACTOR SHALL UTILIZE FORMS AS NECESSARY TO ENSURE THE LSM REMAINS UNDER THE EXISTING ABUTMENT. PLACEMENT SHALL BE IN ACCORDANCE WITH ITEM 613 AND AS INDICATED WITHIN THESE PLANS, SEE STRUCTURE MOT-675-0233. ALL WORK, LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM OF WORK SHALL BE PAID FOR UNDER THE UNIT PRICE BID PER CUBIC YARD FOR ITEM 613 – LOW STRENGTH MORTAR BACKFILL, AS PER PLAN.

ITEM SPECIAL – PATCHING CONCRETE BRIDGE DECK, TYPE B

THIS ITEM OF WORK SHALL BE IN ACCORDANCE WITH PROPOSAL NOTE 512 – ITEM SPECIAL PATCHING CONCRETE BRIDGE DECKS, WITH THE FOLLOWING REVISIONS: REMOVE AND PATCH ONLY UNSOUND PROBLEM AREAS MARKED OUT BY THE ENGINEER IN THE AREA ALONG THE EXISTING BRIDGE JOINTS. THE ENTIRE CONCRETE SURFACE IS NOT TO BE SOUNDED TO REPAIR ALL POTENTIAL UNSOUND AREAS. ALL WORK, LABOR, MATERIAL AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM OF WORK SHALL BE PAID FOR UNDER THE UNIT BID OF SQUARE YARDS FOR ITEM SPECIAL- 519 PATCHING CONCRETE BRIDGE DECK, TYPE B.

THIS ITEM APPLIES ON THE FOLLOWING STRUCTURE(S):  
MOT-4-2050L  
MOT-35-1395L&R

ITEM 516 – BEARING DEVICE, ROCKER, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ALL EXISTING ABUTMENT ROCKER BEARINGS, LOAD PLATES, SHIMS, AND PINS, AND PREPAIR THE AREA FOR NEW BEARINGS. CARE SHALL BE TAKEN NOT TO DAMAGE ANY EXISTING BRIDGE BEAMS, OR ABUTMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING NEW BEARINGS AS INDICATED IN THESE PLANS. ALL WORK, LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM OF WORK SHALL BE PAID FOR AT THE UNIT BID PRICE PER EACH FOR ITEM 516 – BEARING DEVICE, ROCKER, AS PER PLAN.

ITEM 513 – STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN

ITEM 513 – STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN: ALL REQUIREMENTS OF 513 APPLY TO SHOP FABRICATED MEMBERS. PERFORM WORK FOR FIELD FABRICATED MEMBERS ACCORDING TO ITEM 513, EXCEPT AS MODIFIED HEREIN. THE DEPARTMENT WILL NOT REQUIRE THE CONTRACTOR PERFORMING FIELD FABRICATION TO BE PRE-QUALIFIED AS SPECIFIED IN SUPPLEMENT 1078. SUBMIT A WRITTEN LETTER OF MATERIAL ACCEPTANCE IN ACCORDANCE WITH 501.06, TO THE ENGINEER. PROVIDE THE ENGINEER “AS-BUILT” DRAWINGS ACCORDING TO 513.06, EXCEPT 501.04 DOES NOT APPLY. UPON RECEIPT OF THE ENGINEER’S ACCEPTANCE, SUPPLY A COPY OF THE DRAWINGS, ACCORDING TO SUPPLEMENT 1002, TO THE OFFICE OF MATERIAL MANAGEMENT FOR RECORD PURPOSES. PROPOSED STRUCTURAL STEEL MEMBERS, AND AREAS AFFECTED BY THE REPAIR SHALL BE REPAIRED PER CMS 514.22 REPAIR PROCEDURES. THE FINISH COAT SHALL MATCH THE EXISTING STRUCTURE COLOR.

THE FOLLOWING MEMBERS ARE INCLUDED IN THIS ITEM: WEB PERFORATION REPAIRS, STIFFENER PERFORATION REPAIRS, FLANGE REPAIRS/RETROFITS, CROSS FRAME ANGLES, AND END FRAME ANGLES.

ITEM 513 – STRUCTURAL STEEL, MISC.: REPAIR CRACKED WELDS.

CRACKED OR BROKEN WELDS SHALL BE GROUND SMOOTH AND REPAIRED WHERE DETAILED. REPAINTING AREAS AFFECTED BY THE WELD REPAIR SHALL BE REPAIRED PER CMS 514.22 REPAIR PROCEDURES. ALL MATERIALS, LABOR AND EQUIPMENT NEEDED FOR SURFACE PREPARATIONS, WELDING AND PAINT REPAIR SHALL BE INCLUDED FOR PAYMENT WITH ITEM 513 – STRUCTURAL STEEL MISC.; REPAIR CRACKED WELDS.

CLA-4-0611R – WELDING ON END FRAME  
MOT-75-3.73L – MOMENT PLATE WELDS

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ITEM 614, MAINTAINING TRAFFIC

THE CONTRACTOR SHALL PROVIDE, ERECT, AND MAINTAIN SIGNS ( IN PROPER POSITION, CLEAN AND LEGIBLE, AND IN GOOD WORKING CONDITION ) AND REMOVE ALL LIGHTS, SIGNS, CONES, DRUMS, AND ANY OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR MAINTENANCE OF TRAFFIC ACCORDING TO THESE PLAN NOTES AND DETAILS.

PROPOSED STRUCTURE WORK SHALL BE PERFORMED ON STRUCTURES:

- AUG-033-1507L ( W.B. U.S.33 OVER CR25A AND CSX )  
AUG-066-1273 ( S.B./N.B. S.R. 66 OVER U.S. 33 )  
CLA-004-0611R ( S.R. 4 OVER I.R. 70 )  
CLA-054-0561 ( S.R. 54 OVER I.R. 70 )  
CLA-070-0173 ( E.B. & W.B. I.R. 70 OVER MAD RIVER )  
CLA-070-1446 ( SELMA RD. OVER I.R.70 )  
CLA-070-2631 ( SYLVAN RD. OVER I.R.70 )  
CLA-072-0863 ( S.R. 72 OVER MONROE ST, WASHINGTON ST, AND NS )
- CLA-334-0086 ( W.B./E.B. S.R. 334 OVER BIKE PATH AND INDIANA & OHIO URBANA SUB )
- CLA-334-0099 ( W.B./E.B. S.R. 334 OVER S.R. 72 )  
MER-029-1429 L&R ( S.R. 29 OVER R.J. CORMAN R/R )  
MIA-055-1183 ( S.R. 55 OVER GREAT MIAMI RIVER )  
MOT-004-2050L ( S.R. 4 OVER HARSHMAN ROAD )  
MOT-035-1395 L&R ( S.R. 35 OVER FRONTAGE ST, B&O, AND HOME AVE. )
- MOT-035-1629R ( S.B. 35 OVER S. PATTERSON BLVD. AND S.R. 48 )
- MOT-048-1214 ( LUDLOW ST., RAMP "D" OVER S.R. 48 )  
MOT-049-1091L ( S.R. 49 OVER I.R. 70 )  
MOT-075-0373L ( I.R 75 OVER S.R. 725 )  
MOT-075-1466 ( I.R.75 OVER LEO ST )  
MOT-075-1503 ( I.R. 75 OVER STANLEY AVE. )  
MOT-675-0063R ( I.R. 675 OVER S.R.741 AND TRIB OF HOLES CR. )
- MOT-675-0129 ( LYONS RD. OVER I.R. 675 )  
MOT-675-0233 ( YANKEE ST. OVER I.R. 675 )  
MOT-675-0647 ( BIGGER RD. OVER I.R. 675 )

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF DESCRIBED IN CMS 108.07 FOR EACH DAY THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

PERMITTED LANE CLOSURE

THE LANE CLOSURE TIMES ON INTERSTATE SYSTEM ROADWAYS SHALL BE IN ACCORDANCE WITH SET PERMITTED LANE CLOSURE TIMES. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THE PERMITTED LANE CLOSURE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$75 FOR EACH MINUTE THE LANE CLOSURE RESTRICTIONS ARE VIOLATED.

LANE CLOSURES SHALL ONLY BE IMPLEMENTED AT THE TIMES TIMES LISTED ON THE OHIO DEPARTMENT OF TRANSPORTATION'S PERMITTED LANE CLOSURE WEB SITE WHICH IS LOCATED AT:

HTTP://PLCM.DOT.STATE.OH.US

THE PERMITTED CLOSURE TIMES LISTED ON THE WEBSITE, FOURTEEN (14) CALENDAR DAYS PRIOR TO THE BID LETTING DATE, SHALL BE IN EFFECT FOR THIS PROJECT.

NO WORK WITHIN ACTIVE TRAVEL LANES OR WHICH WILL SLOW TRAFFIC IS PERMITTED AT ANY OTHER TIMES. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$75 FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO DT PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HRS & < 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	>= 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13),SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	>= 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HRS & < 2 WKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

RAMP WILL BE  
CLOSED MMM-DD  
FOR        DAYS  
INFO: 1-888-200-9919

W20-H13-60

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN) (continued)

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614, MAINTAINING TRAFFIC

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 10 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, DETOUR SIGNING

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SETTING THE DETOUR SIGNING AS INDICATED IN THESE PLANS. IF PLACEMENT IS SET IN ADVANCE OF THE NOTICE OF CLOSURE SIGN TIME TABLE, THE SIGNS SHALL BE COVERED. ONCE ALL WORK HAS BEEN COMPLETED THE CONTRACTOR SHALL REMOVE THE DETOUR SET IN PLACE.

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ECT.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

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FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 2 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (Continued)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 3 SIGN MONTH ASSUMING 2 PCMS SIGN(S) FOR 6 MONTH(S)

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE, AS PER PLAN.

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN A NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE, AS PER PLAN. (CON'T.)

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING THE SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 100 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.



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LOCATION SPECIFIC MOT:

AUG-033-1507L ( W.B. U.S.33 OVER CR25A AND CSX )

US 33: ACCEPTABLE MOT SHALL EITHER BE A) TO MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES IN THE MEDIAN BEHIND THE EXISTING GUARDRAIL AS DETAILED ON SHEET 9, OR B) USING SCD MT-95.30 TO CLOSE THE INSIDE LANE DURING WORK HOURS ONLY.

AUG-066-1273 ( S.B./ N.B. S.R. 66 OVER U.S. 33 )

US-33: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. THE OUTSIDE SHOULDERS MAY BE CLOSED USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITON, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

SR-66: THE OUTSIDE NORTHBOUND LANE ONLY MAY BE CLOSED USING SCD MT-95.31 DURING WORK HOURS ONLY.

CLA-004-0611R ( S.R. 4 OVER I.R. 70)

IR-70: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. THE OUTSIDE SHOULDERS MAY BE CLOSED USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITION, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

SR-4: ONE LANE MAY BE CLOSED USING SCD MT-95.30 DURING WORK HOURS ONLY.

CLA-054-0561 ( S.R. 54 OVER I.R. 70 )

IR-70: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. THE OUTSIDE SHOULDERS MAY BE CLOSED USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITION, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

SR-54: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

CLA-070-0173 ( E.B. & W.B. I.R. 70 OVER MAD RIVER )

IR-70: CLOSE THE OUTSIDE SHOULDERS USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITION, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

CLA-070-1446 ( SELMA RD. OVER I.R.70 )

IR-70: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. THE OUTSIDE SHOULDERS MAY BE CLOSED USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITON, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

SELMA PIKE: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

CLA-070-2631 ( SYLVAN SHORES RD. OVER I.R.70 )

IR-70: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. THE OUTSIDE SHOULDERS MAY BE CLOSED USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITON, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

SELMA PIKE: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

CLA-072-0863 ( S.R. 72 OVER MONROE ST. WASHINGTON ST. AND NS RR.)

IR-70: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

MONROE & WASHINGTON ST.: CLOSE 1 LANE USING MT-97.10. ALL EQUIPMENT, MATERIAL, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PEDESTRIAN AND BIKE TRAFFIC DURING WORK HOURS.

CLA-334-0086 ( W.B./ E.B. S.R. 334 OVER BIKE PATH AND INDIANA & OHIO URBANA SUB RR.)

SR-334: CLOSE THE OUTSIDE SHOULDERS USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITION, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USE TO PROTECT THE WORK AREA.

CLA-334-0099 ( W.B./ E.B. S.R. 334 OVER S.R. 72 )

SR-70: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. THE OUTSIDE SHOULDERS MAY BE CLOSED USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITION, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

SR-334: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

MER-029-1429 ( S.R. 29 OVER R.J. CORMAN RAILROAD )

US 33: ACCEPTABLE MOT SHALL EITHER BE A) TO MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES IN THE MEDIAN BEHIND THE EXISTING GUARDRAIL AS DETAILED ON SHEET 9, OR B) CLOSING ONE LANE USING SCD MT-95.30 DURING WORK HOURS.

MIA-055-1183 ( S.R. 55 OVER GREAT MIAMI RIVER )

SR-55: CLOSE THE OUTSIDE NB LANE USING MT-95.31 DURING WORK HOURS ONLY. ALL EQUIPMENT, MATERAIL, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER. DETOUR PEDESTRIANS AS SHOWN ON SHEET 10,11.

MOT-004-2050L ( S.R. 4 OVER HARSHMAN ROAD )

SR-4: CLOSE LANES USING MT-95.30 DURING WORK HOURS ONLY. ALL EQUIPMENT, MATERAIL, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER. DETOUR THE ON RAMP FROM HARSHMAN RD AS SHOWN ON SHEET 17 WHEN THE OUTSIDE LANE ON SB SR -4 IS CLOSED ONLY.

HARSHMAN RD.: MAINTAIN ALL THROUGH LANES OF TRAFFIC AT ALL TIMES, TURN LANES TO RAMP FROM HARSHMAN RD SHALL BE DETOURED AS INDICATED ABOVE.

MOT-035-1395 L&R ( U.S. 35 OVER FRONTAGE ST, B&O RR. AND HOME AVE.)

US-35: CLOSE LANES AS DETAILED ON SHEETS 16-20 AND USING MT-95.30 & 98.11 DURING WORK HOURS ONLY. ALL EQUIPMENT, MATERIALS, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER. SINGLE LANE CLOSURE LEFT ANYTIME - TWO LANE CLOSURE RIGHT - 8:PM TO 2:PM.

FRONTAGE ST. & HOME AVE.: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

MOT-035-1629R ( S.B. 35 OVER S. PATTERSON BLVD. AND S.R. 48 )

US-35: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

SR-48: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. DETOUR PEDESTRIAN AS SHOWN ON SHEET 22.

MOT-048-1214 ( LUDLOW ST. RAMP "D" OVER S.R. 48 )

US-35: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

SR-48: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. DETOUR PEDESTRIAN AS SHOWN ON SHEET 22.

MOT-049-1091L ( S.R. 49 OVER I.R. 70 )

SR-70: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES IN BEHIND THE EXISTING SHOULDER PIER PROTECTION. THE OUTSIDE SHOULDERS MAY BE CLOSED USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITION, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

SR-49: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

MOT-075-0373L ( I.R 75 SB. OVER S.R. 725 )

IR-75: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

SR-725: SEE DETAILS ON SHEETS 24-28.

ALL EQUIPMENT, MATERIALS, AND VEHICLES SHALL BE STAGED WITHIN THESE CLOSURES, UNLESS APPROVED BY THE ENGINEER.

MOT-075-1466 ( I.R.75 OVER LEO ST )

IR-75: CLOSE LANES USING MT-95.30 DURING WORK HOURS ONLY. ALL EQUIPMENT, MATERAIL, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER.

LEO ST.: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES. MAINTAIN PEDESTRIAN WALKWAYS AT ALL TIMES. ALL EQUIPEMENT, MATERIAL, AND VEHICLES SHALL BE STAGED BEHIND THE CURB, FENCING, ALONGSIDE HILLROSE AVE, OR ALONGSIDE THE ALLEY UNLESS APPROVED BY THE ENGINEER.

MOT-075-1503 ( I.R. 75 OVER STANLEY AVE.)

IR-75: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

STANLEY AVE: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES BY STAGING EQUIPMENT, MATERIALS, AND VEHICLES BEHIND THE EXISTING SHOULDER PIER PROTECTION. DETOUR PEDESTRIAN AS SHOWN ON SHEET 29.

MOT-235-0022L ( SR 235 OVER SR-4 )

SR-235: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

SR-4: CLOSE OUTSIDE SB LANES USING MT-95.30 DURING WORK HOURS ONLY. ALL EQUIPMENT, MATERIAL, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER.

CLOSE TURN LANES ON SR-235 TO THE RAMP FOR SB SR-4. AND DETOUR THE RAMP TO SB SR-4 FROM SR-235 AS SHOWN ON SHEET 36 DURING WORK HOURS ONLY.

MOT-675-0063R ( RAMP "V" OVER S.R.741 AND TRIB OF HOLES CR.)

IR-675: CLOSE ONE LANE USING MT-95.30 DURING WORK HOURS ONLY. ALL EQUIPMENT, MATERIALS, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER.

SR-741: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

MOT-675-0129 ( LYONS RD. OVER I.R. 675 )

IR-675: CLOSE THE OUTSIDE NB & SB LANE USING MT-95.30 DURING WORK HOURS ONLY. ALL EQUIPMENT, MATERIALS, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER. PLCS: 7PM to 6AM, SUNDAY NIGHT THRU FIRDAY MORNING.

LYONS RD.: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

MOT-675-0233 ( YANKEE ST. OVER I.R. 675 )

IR-675: CLOSE THE OUTSIDE NB & SB LANE USING MT-95.30 DURING WORK HOURS ONLY. ALL EQUIPMENT, MATERIALS, AND VEHICLES SHALL BE STAGED WITHIN THIS CLOSURE, UNLESS APPROVED BY THE ENGINEER.

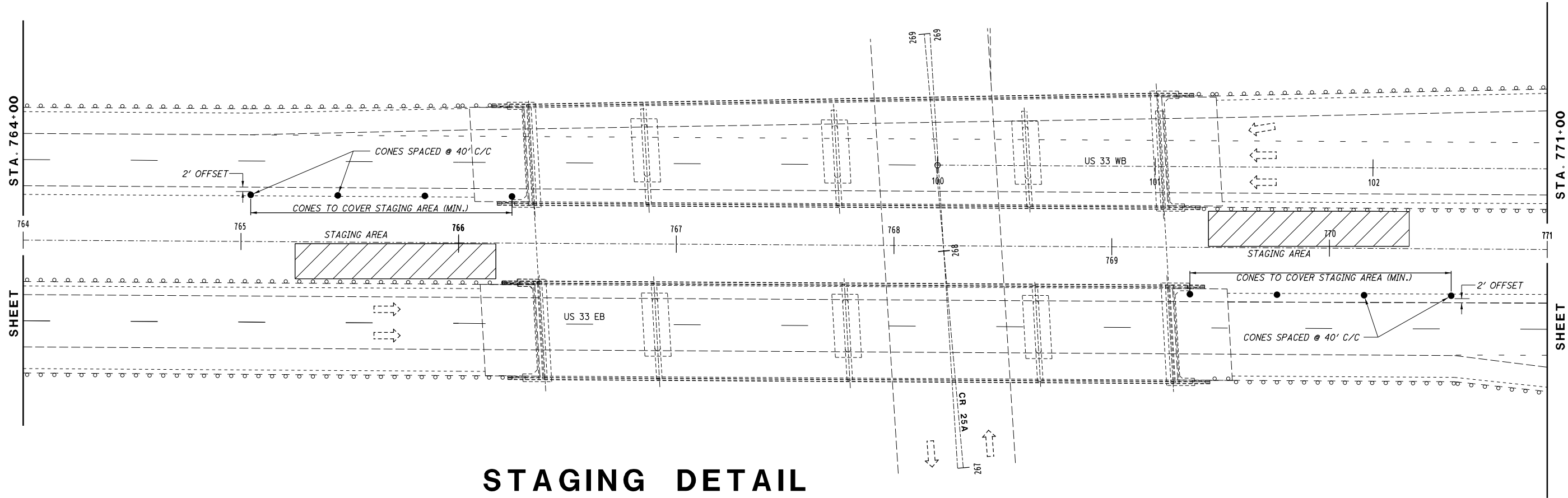
YANKEE ST.: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

MOT-675-0647 ( BIGGER RD. OVER I.R. 675 )

IR-675: CLOSE THE OUTSIDE SHOULDERS USING MT-95.45 DURING WORK HOURS ONLY. BARRELS SHALL BE USED INSTEAD OF PORTABLE BARRIER. IN ADDITION, A SHADOW VEHICLE WITH A TRUCK MOUNTED ATTENUATOR IN ACCORDANCE WITH CMS 614.03 SHALL BE USED TO PROTECT THE WORK AREA.

BIGGER RD.: MAINTAIN ALL LANES OF TRAFFIC AT ALL TIMES.

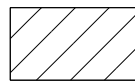
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# STAGING DETAIL

( AUG-33-1507L SHOWN/MER-29-1427 L&R SIMILAR)

## LEGEND



= STAGING AREA



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HORIZONTAL  
SCALE IN FEET

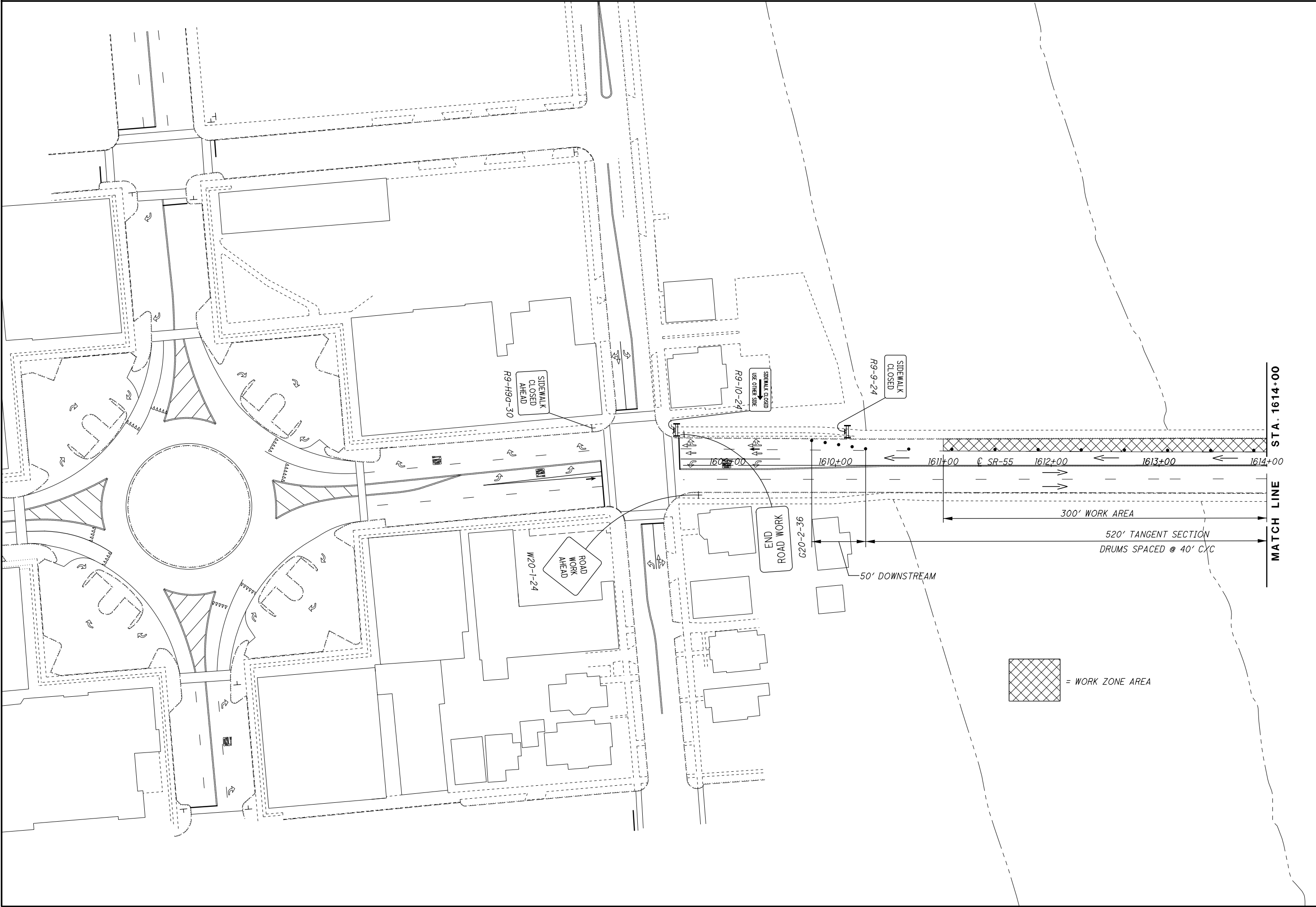
CALCULATED  
REB  
CHECKED  
PNS

MAINTENANCE OF TRAFFIC PLAN  
MEDIAN STAGING DETAILS

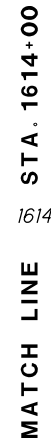
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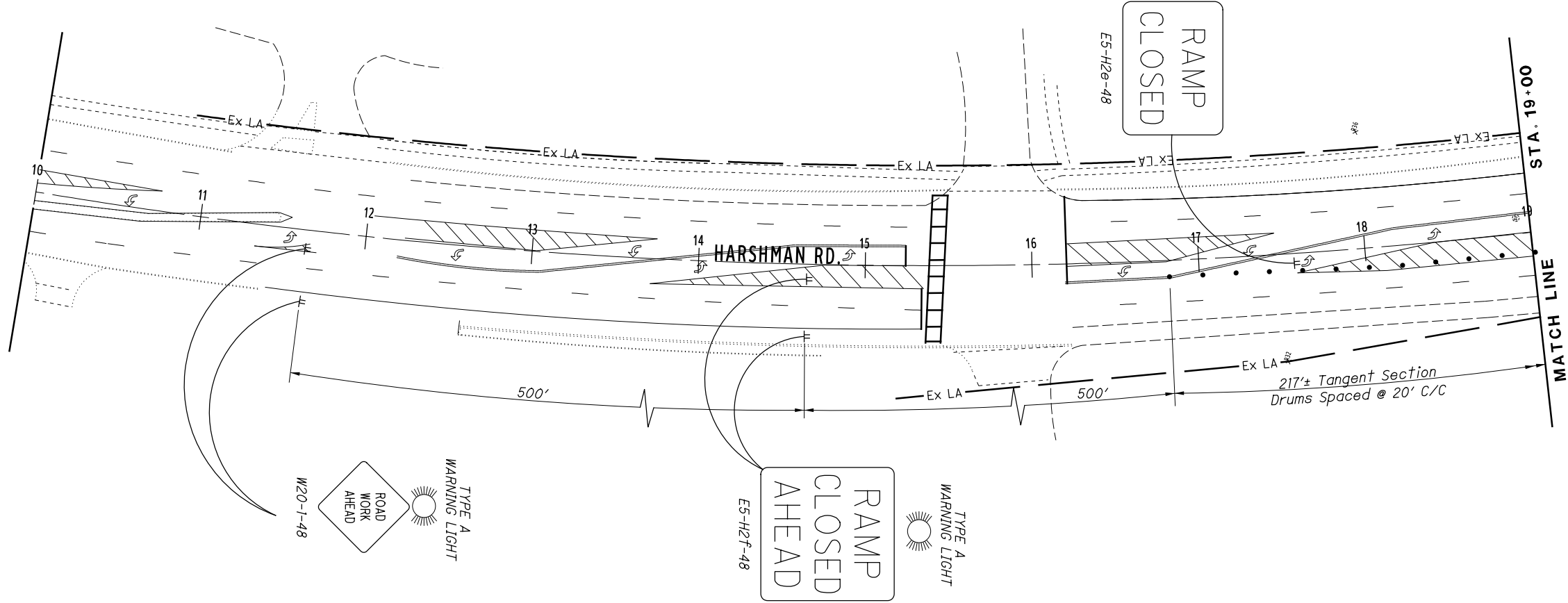
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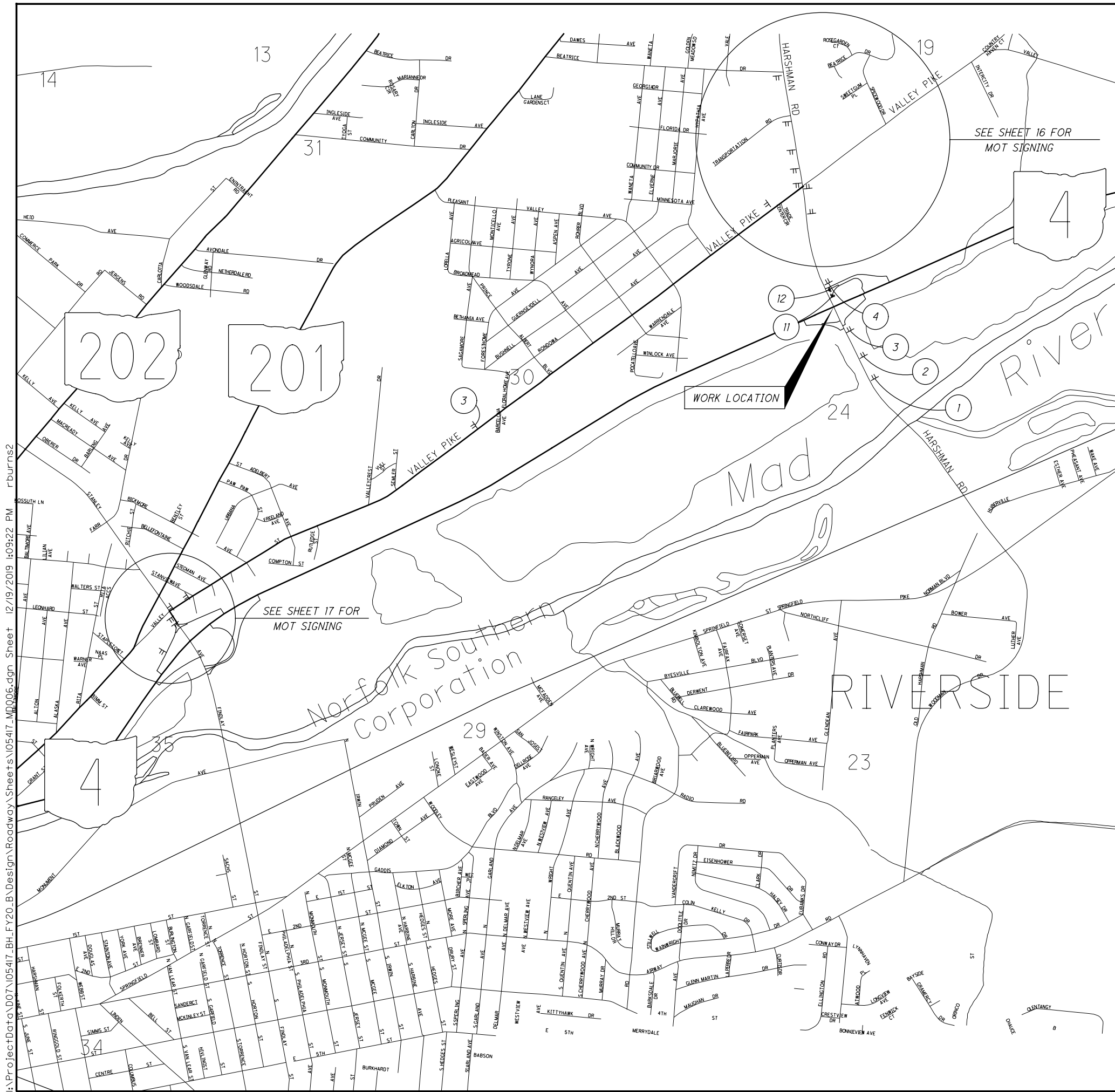






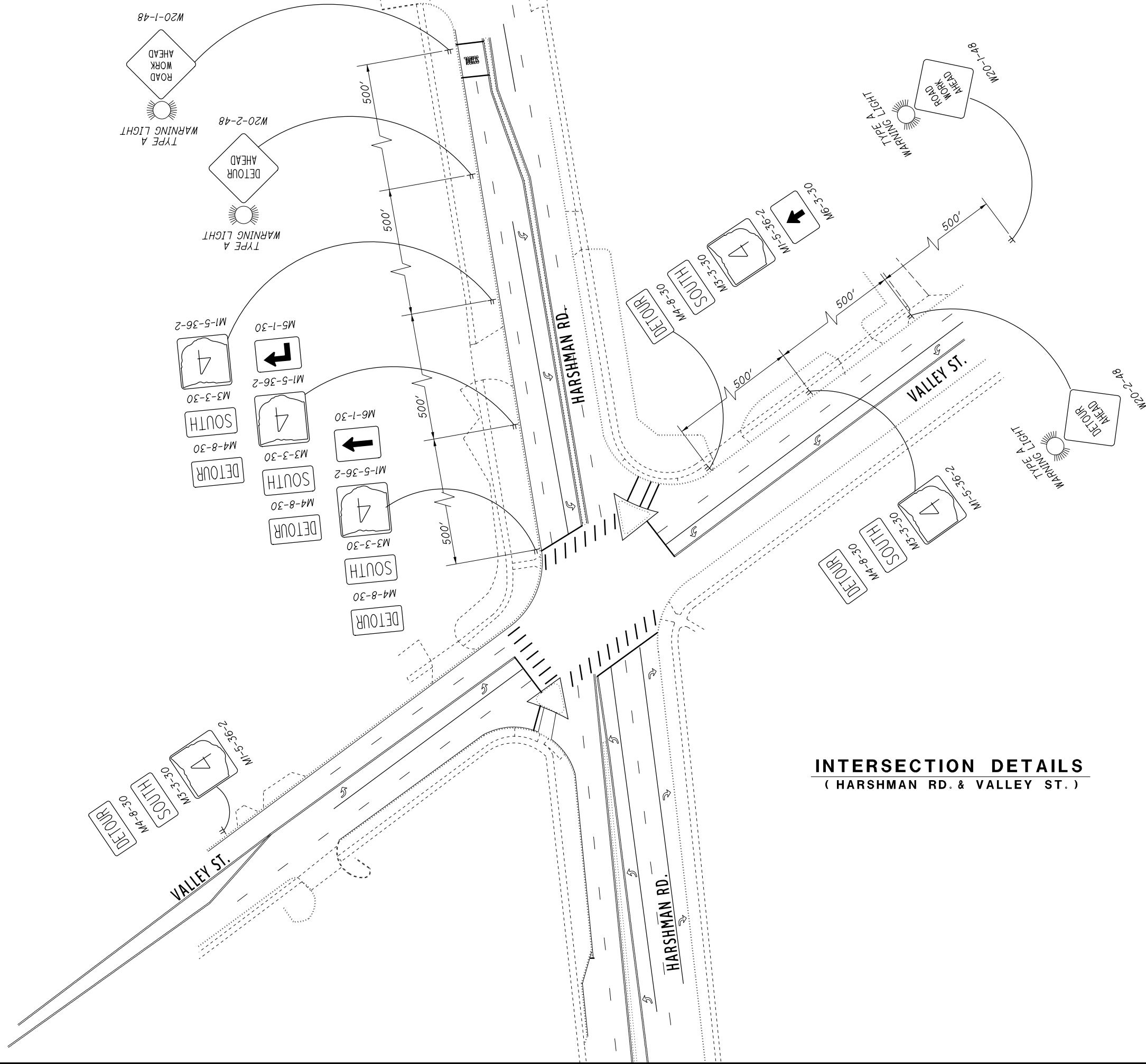


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<p>TYPE A WARNING LIGHT</p> <p>ROAD WORK AHEAD</p> <p>W20-1-48 1 MILES W16-3aP-30 1</p>	<p>TYPE A WARNING LIGHT</p> <p>DETOUR AHEAD</p> <p>W20-2-48 2</p>	<p>DETOUR M4-8-30 SOUTH M3-3-30 4 M1-5-36-2 3</p>	<p>DETOUR M4-8-30 SOUTH M3-3-30 4 M1-5-36-2 ↑ M6-3-30 4</p>	
<p>DETOUR M4-8-30 SOUTH M3-3-30 4 M1-5-36-2 ← M5-1-30 5</p>	<p>DETOUR M4-8-30 SOUTH M3-3-30 4 M1-5-36-2 ← M6-1-30 6</p>	<p>DETOUR M4-8-30 SOUTH M3-3-30 4 M1-5-36-2 ↗ M5-1-30 7</p>	<p>DETOUR M4-8-30 SOUTH M3-3-30 4 M1-5-36-2 → M6-1-30 8</p>	<p>DETOUR M4-8-30 SOUTH M3-3-30 4 M1-5-36-2 ↘ M5-2-30 9</p>
<p>END DETOUR M4-8a-24 10</p>	<p>TYPE B FLASHING ( YELLOW ) WARNING LIGHT</p> <p>ROAD CLOSED</p> <p>R11-2-48 ON TYPE III BARRICADE 11</p>	<p>TYPE B FLASHING ( YELLOW ) WARNING LIGHT</p> <p>ROAD CLOSED</p> <p>R11-2-48 M4-10L-48 ON TYPE III BARRICADE 12</p>		

FOR MORE DETAILS SEE MAINTENANCE  
OF TRAFFIC SHEETS 12-14.

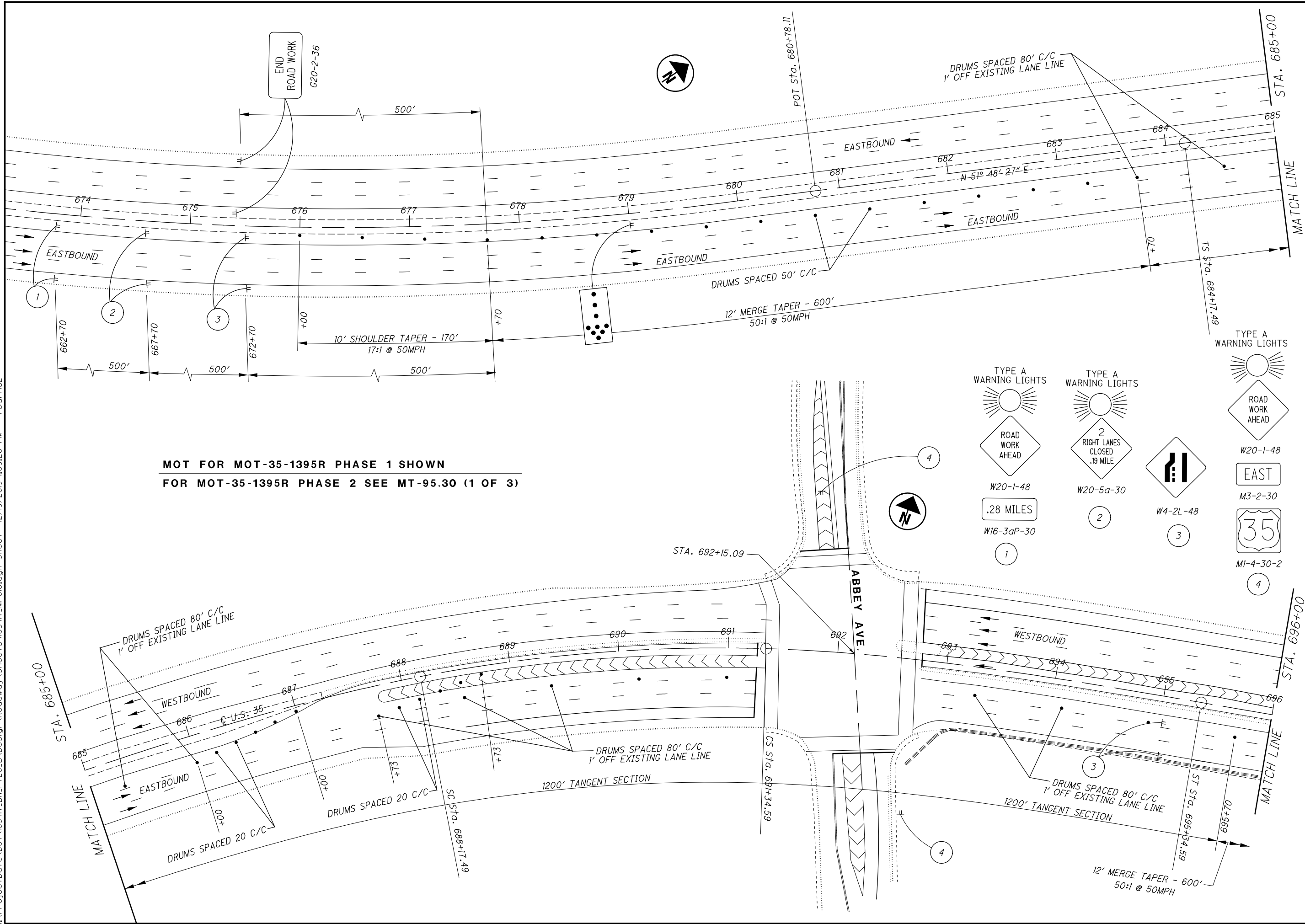


**INTERSECTION DETAILS**  
( HARSHMAN RD. & VALLEY ST. )

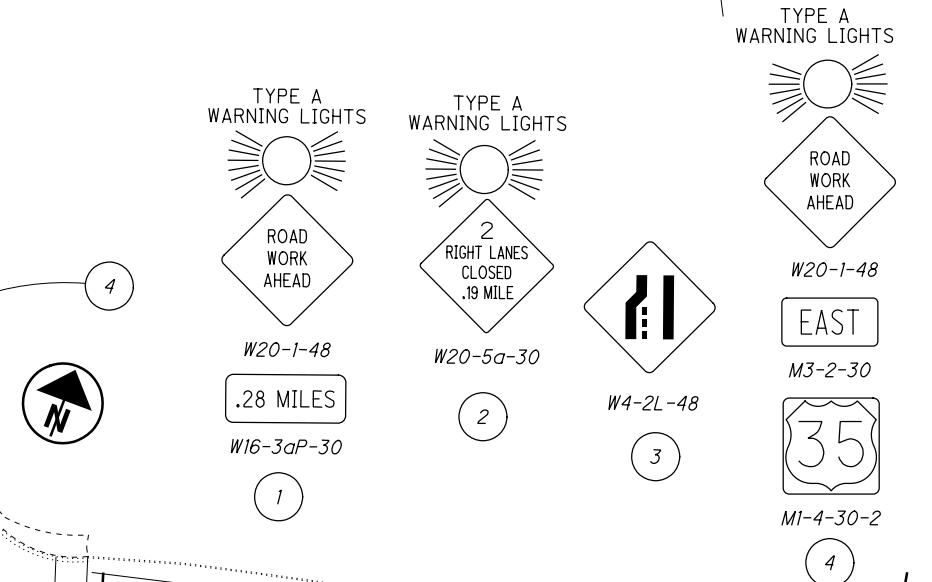




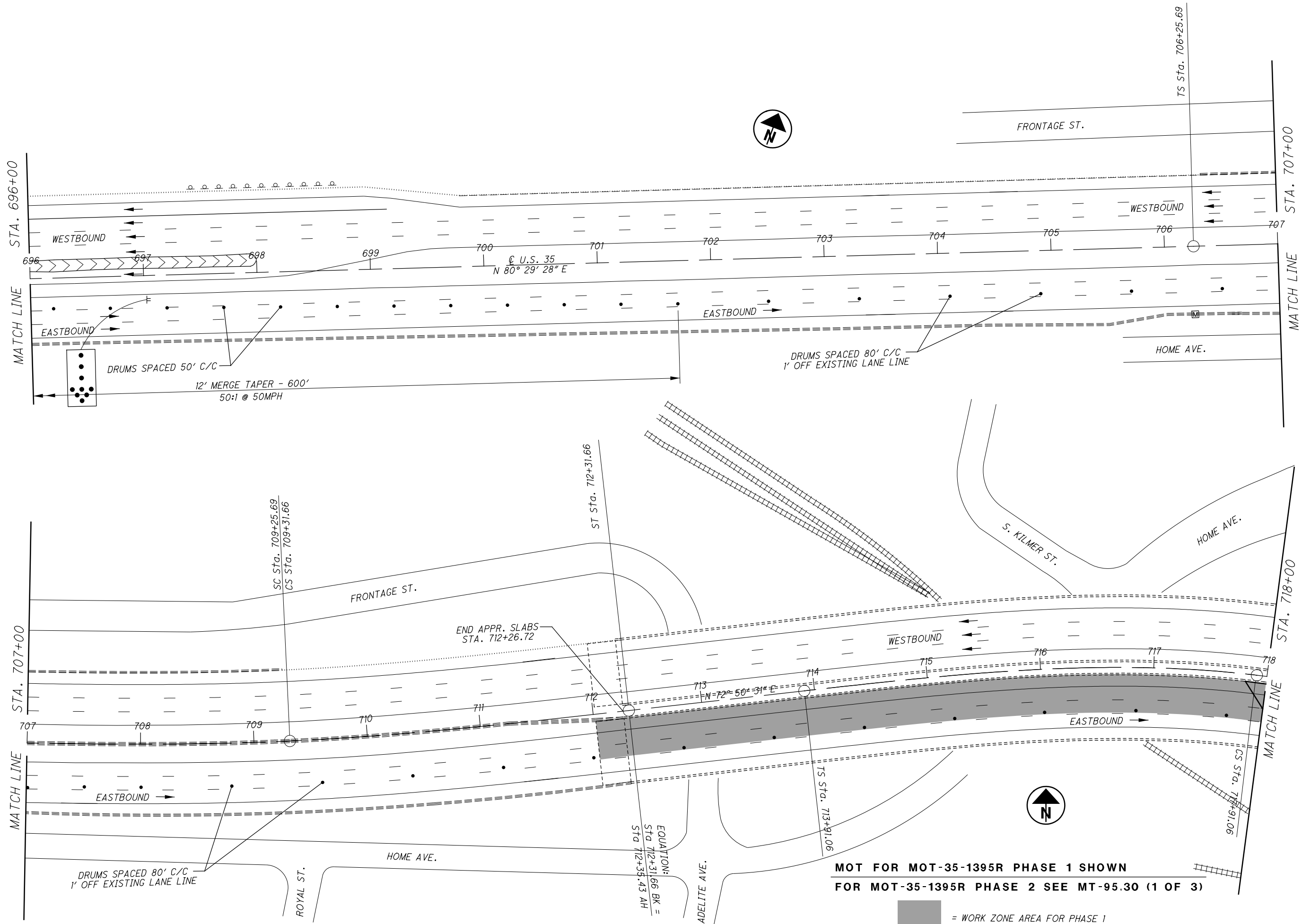
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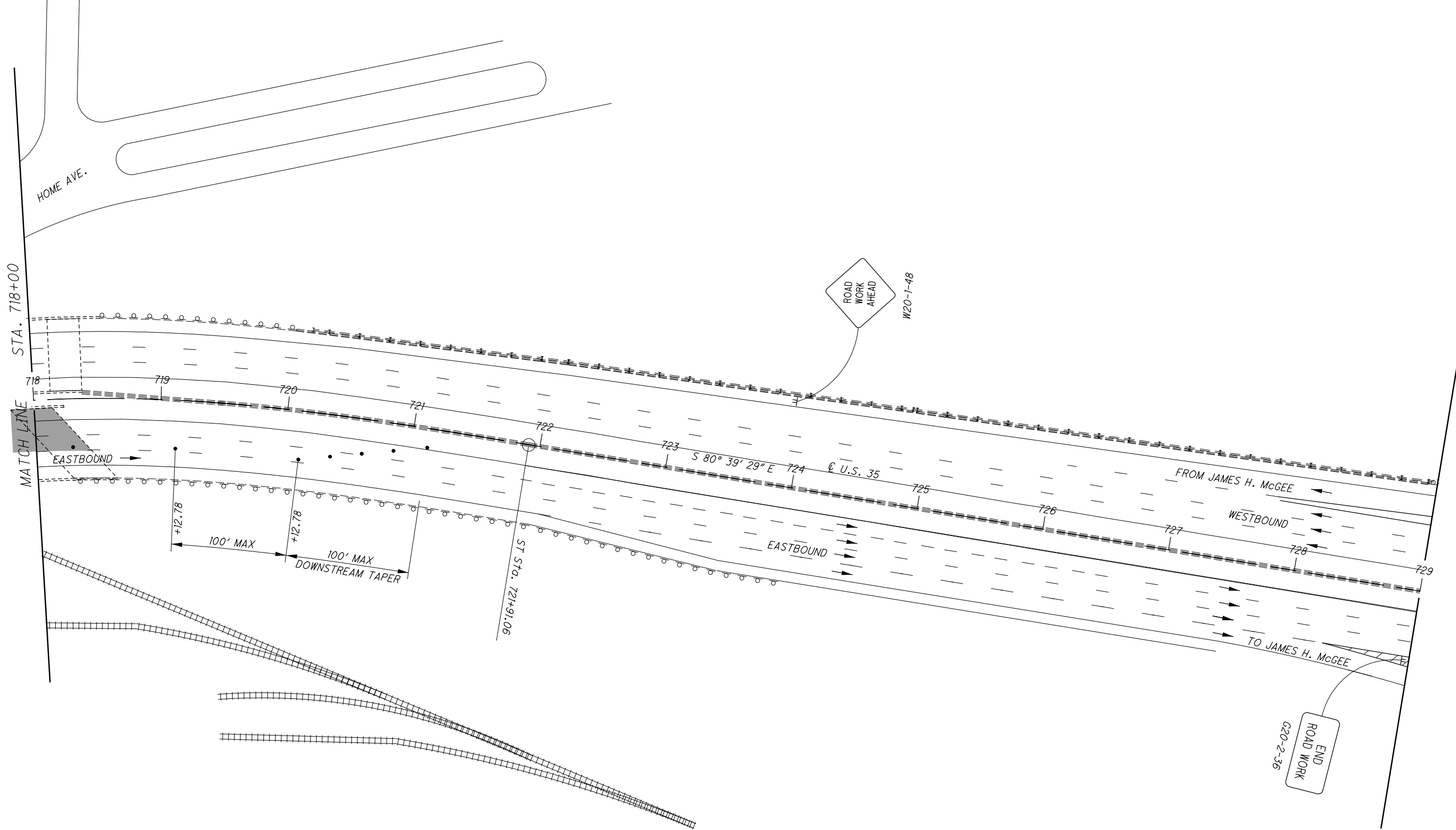
MOT FOR MOT-35-1395R PHASE 1 SHOWN  
FOR MOT-35-1395R PHASE 2 SEE MT-95.30 (1 OF 3)



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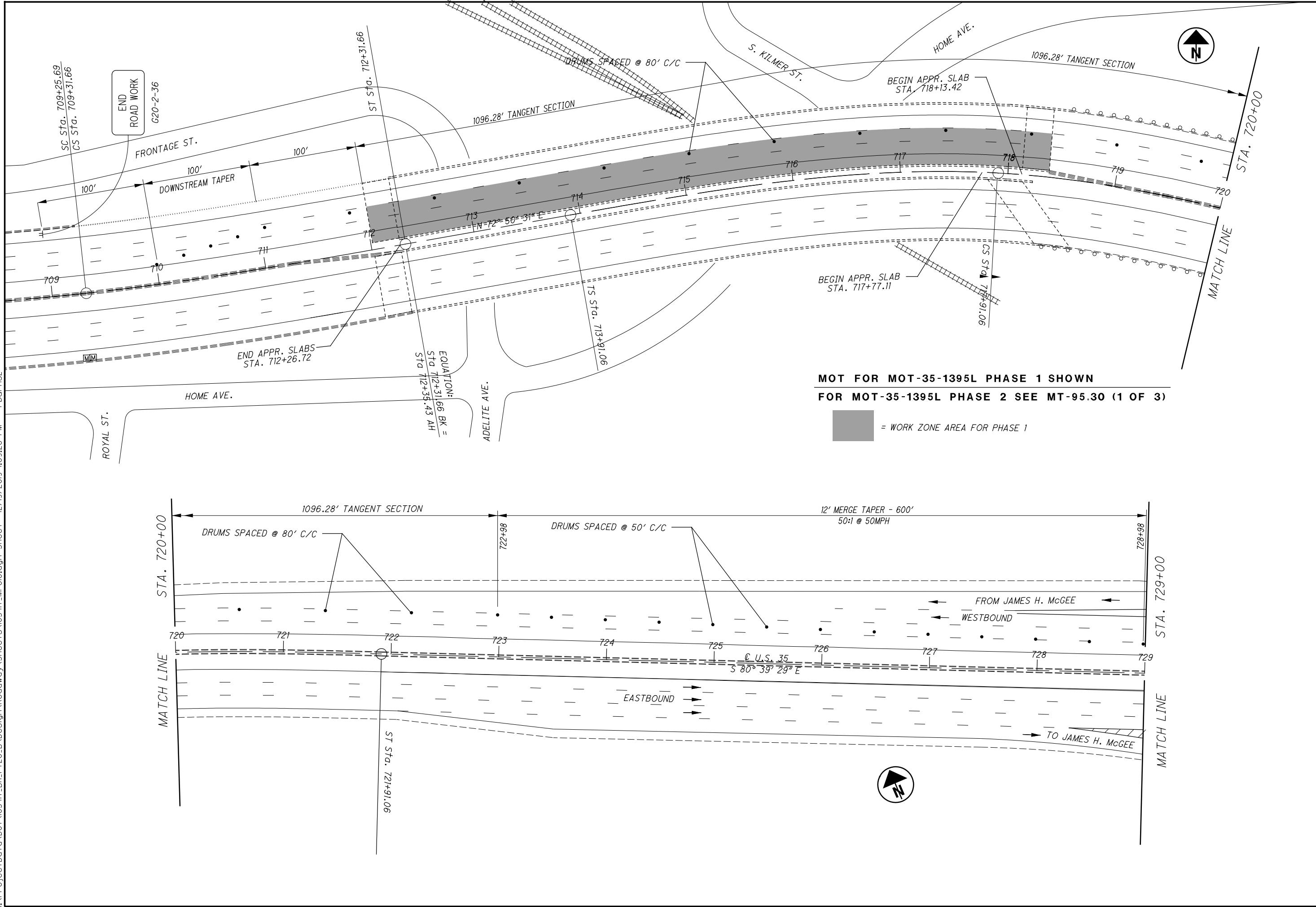


**MOT FOR MOT-35-1395R PHASE 1 SHOWN**  
**FOR MOT-35-1395R PHASE 2 SEE MT-95.30 (1 OF 3)**

 = WORK ZONE AREA FOR PHASE 1

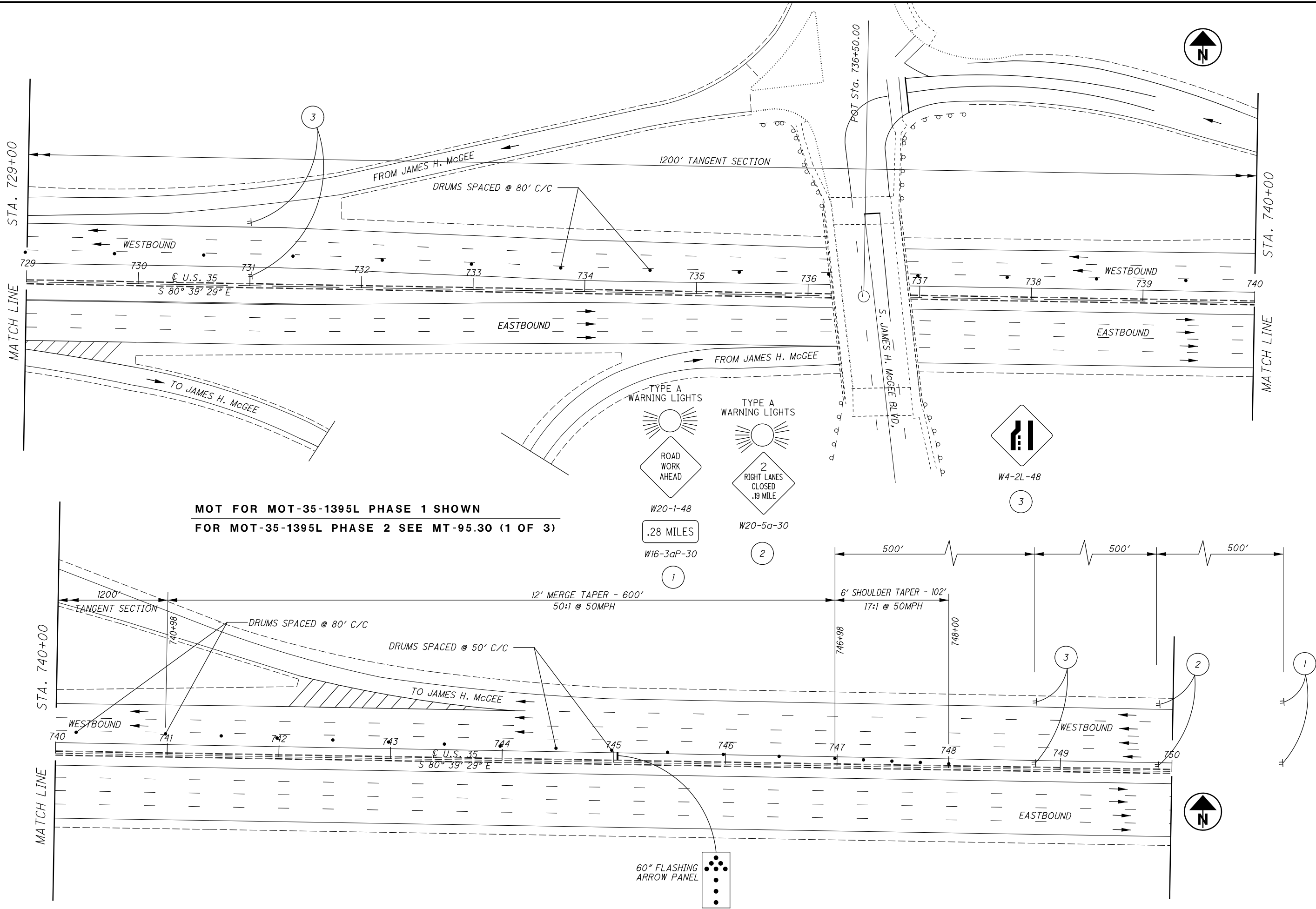
		MAINTENANCE OF TRAFFIC - PHASE 1 (3 OF 3)	
		CALCULATED REB	CHECKED PNS
D07-BH-FY20(B)		MOT-35-1395R OVER FRONTAGE AND HOME AVE.	
20 67			

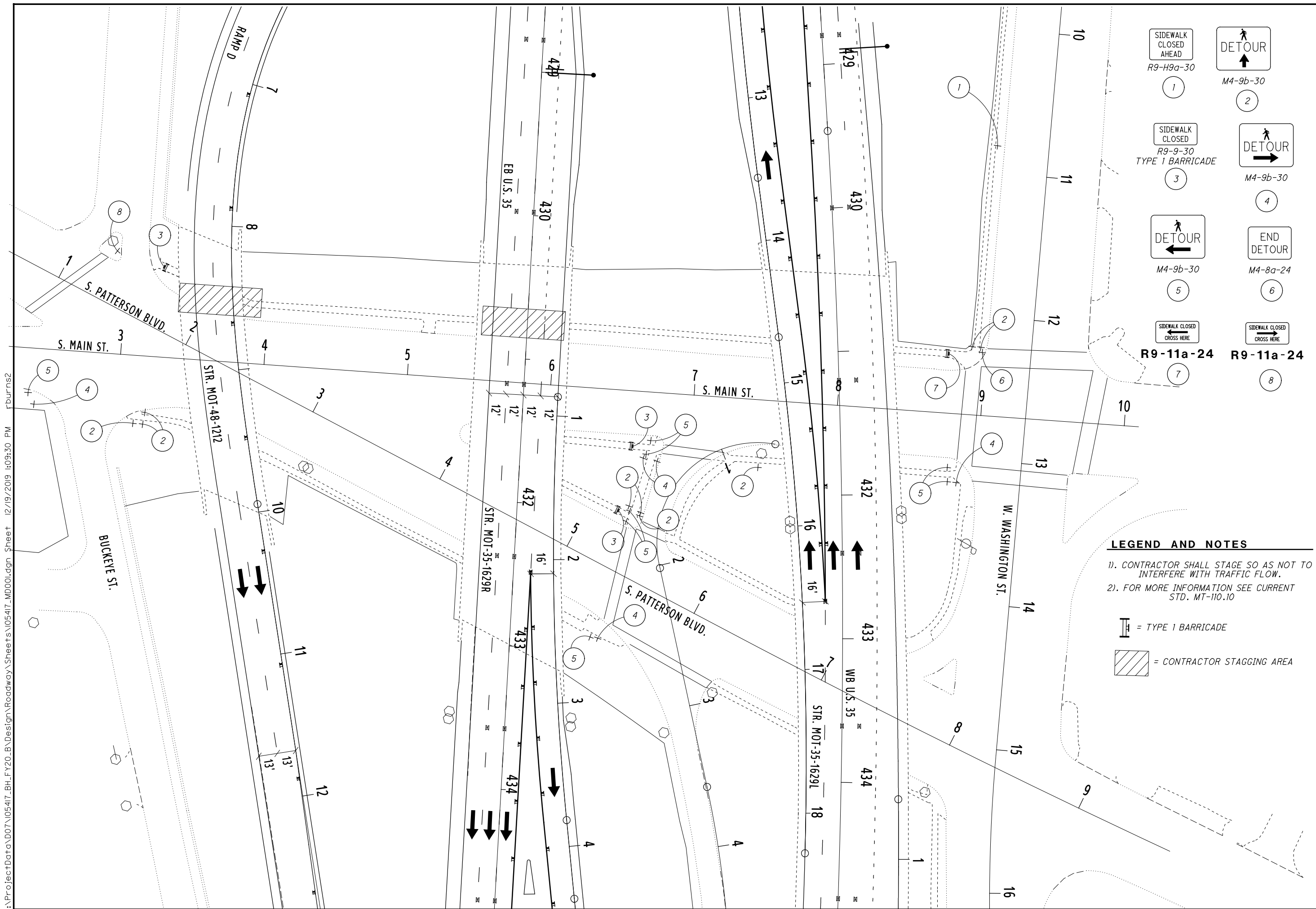
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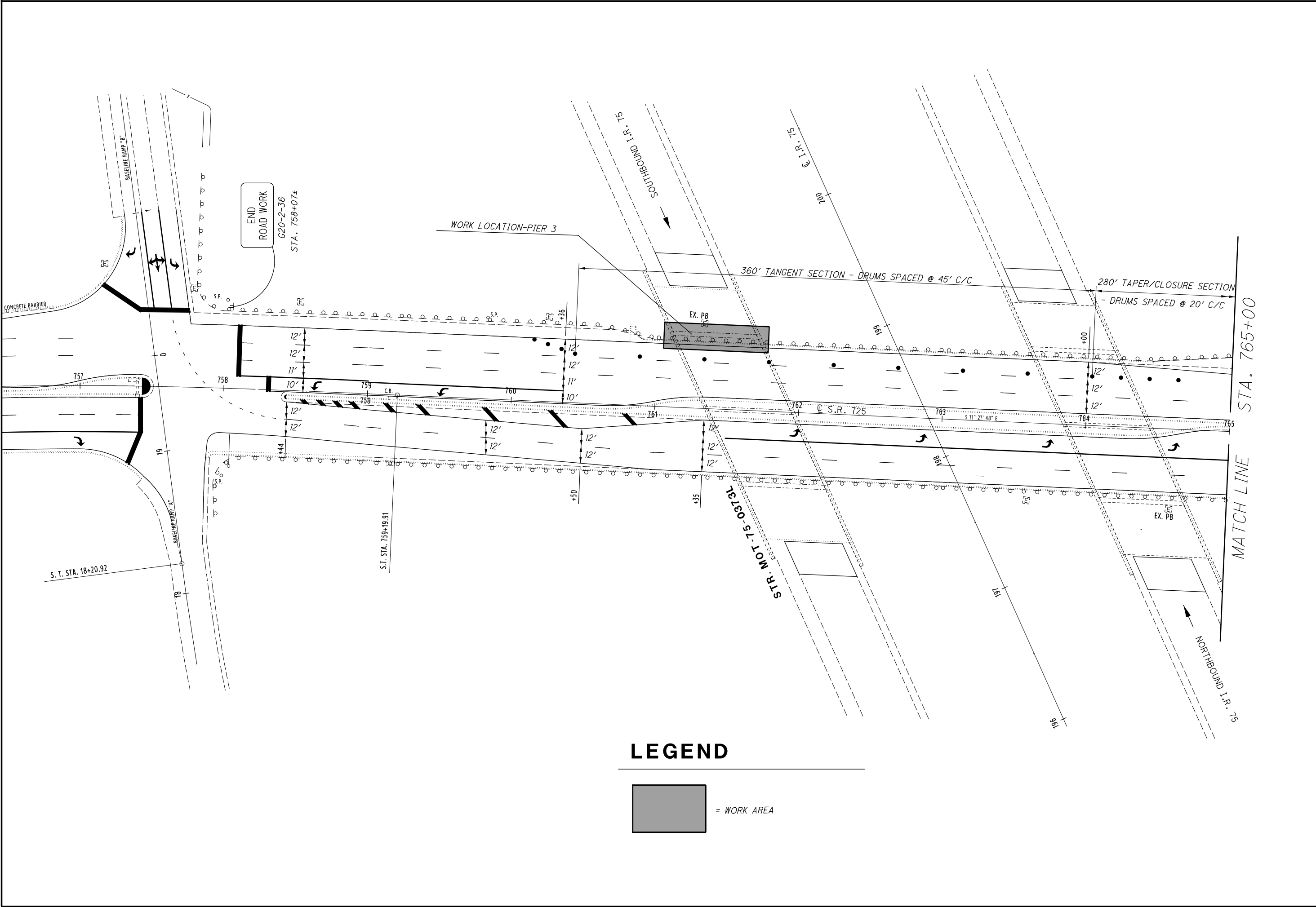


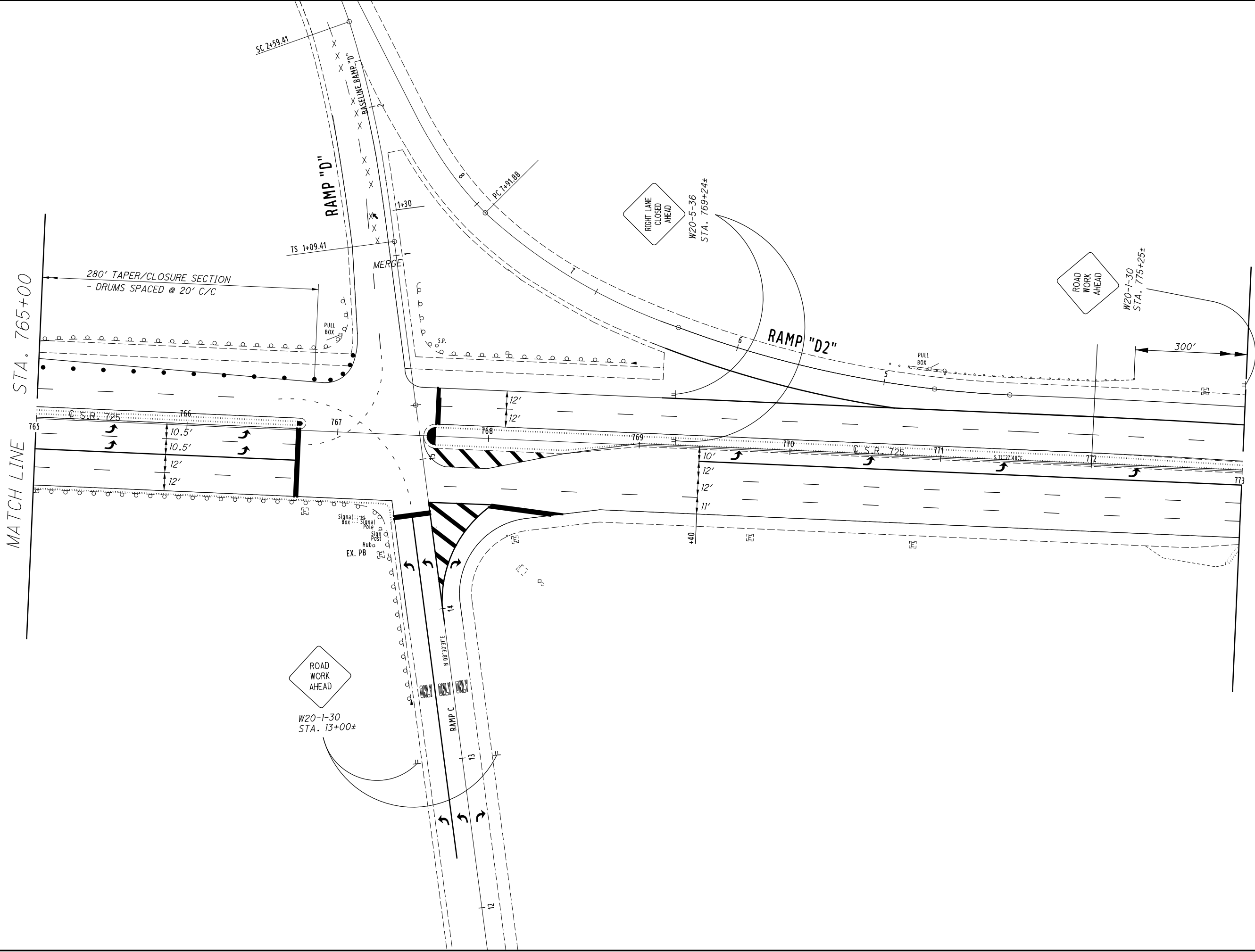
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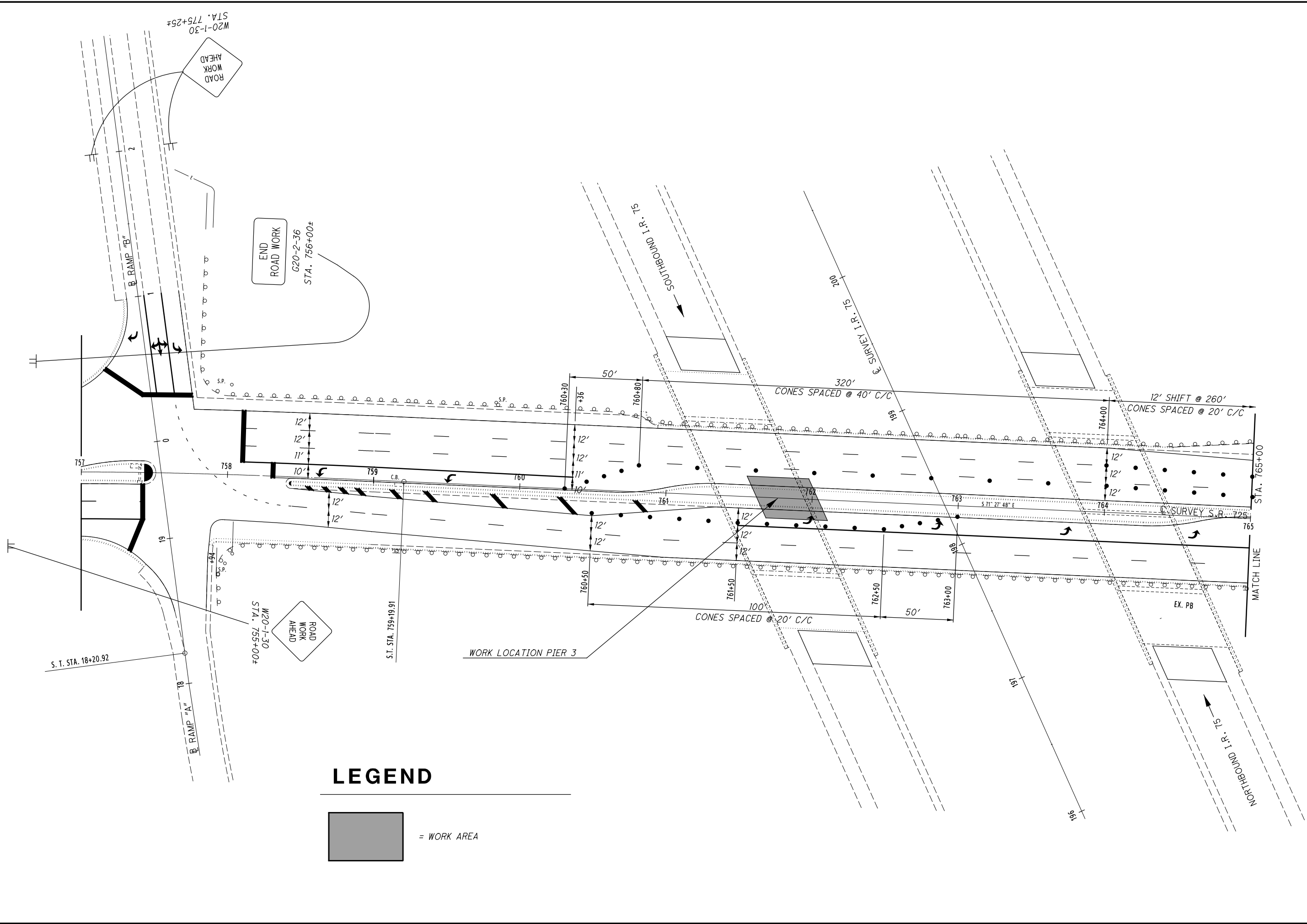




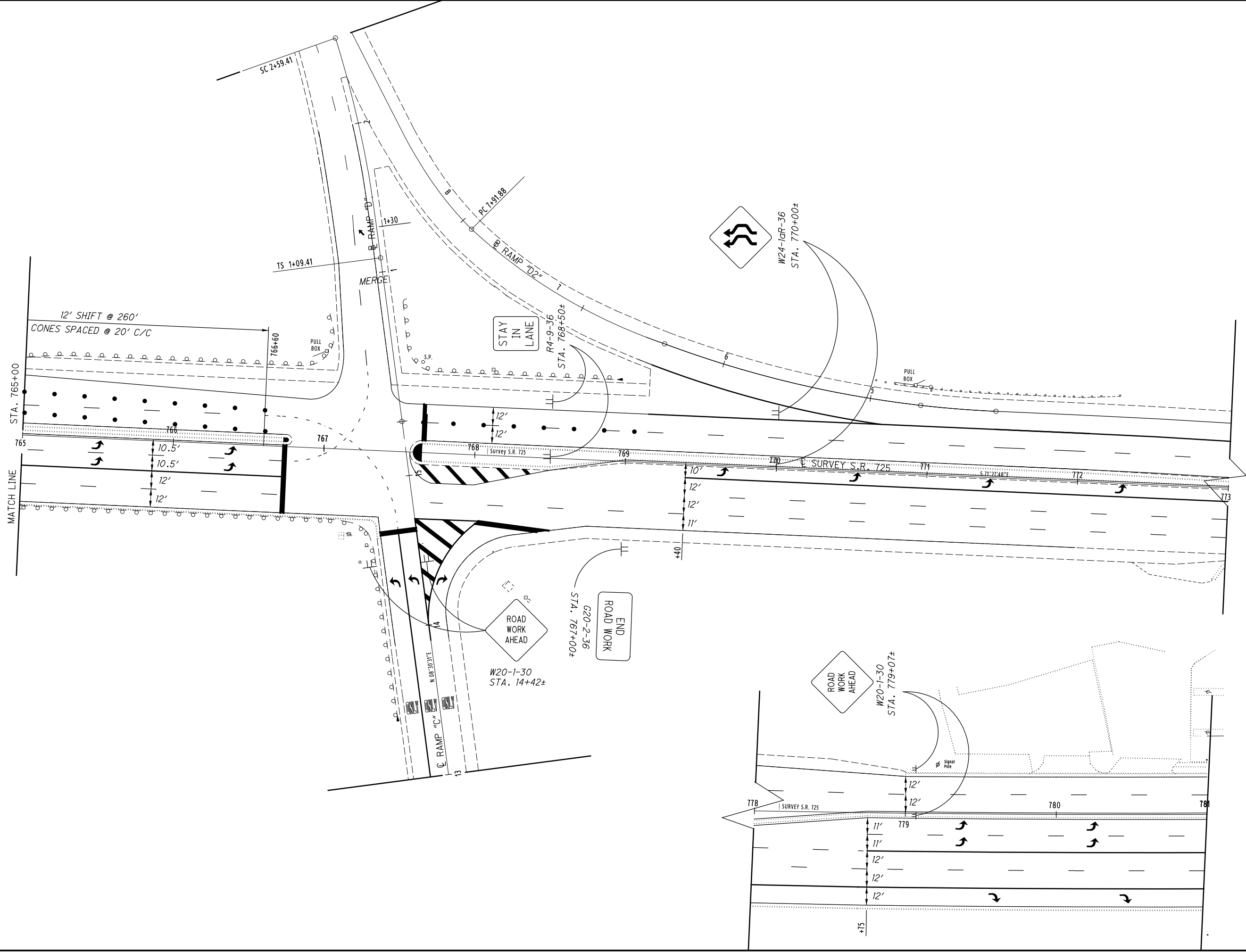
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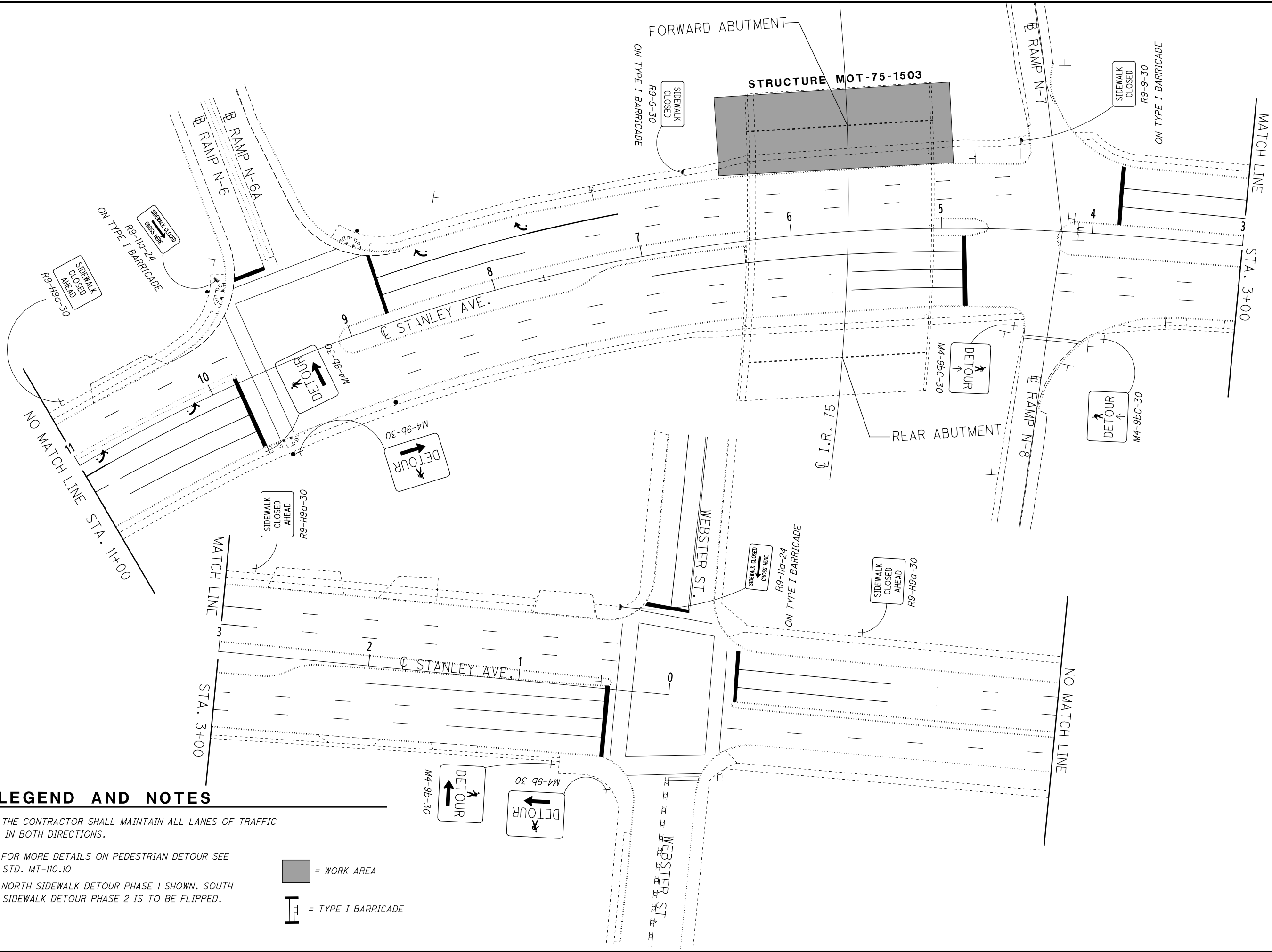
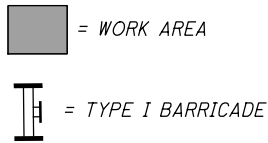




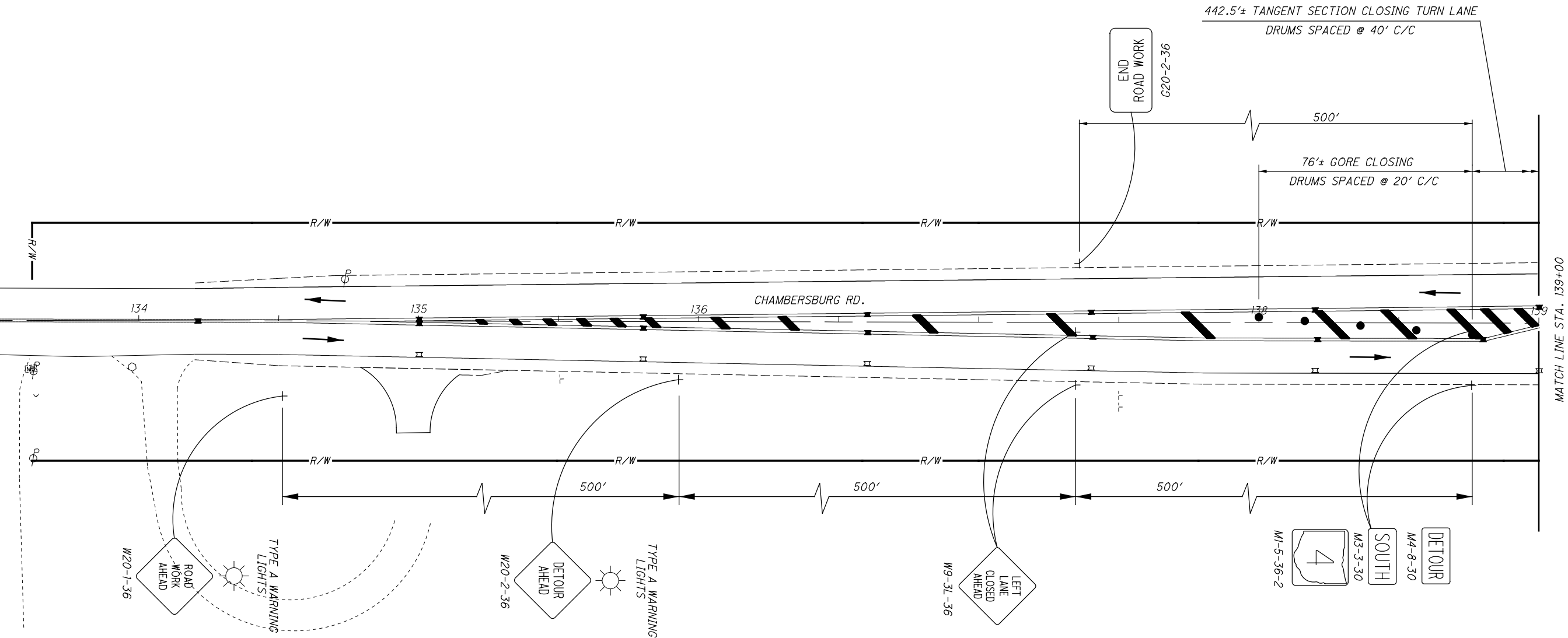
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## LEGEND AND NOTES

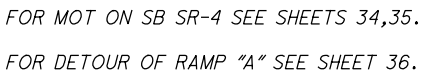
- 1). THE CONTRACTOR SHALL MAINTAIN ALL LANES OF TRAFFIC IN BOTH DIRECTIONS.
- 2). FOR MORE DETAILS ON PEDESTRIAN DETOUR SEE STD. MT-110.10
- 3). NORTH SIDEWALK DETOUR PHASE 1 SHOWN. SOUTH SIDEWALK DETOUR PHASE 2 IS TO BE FLIPPED.



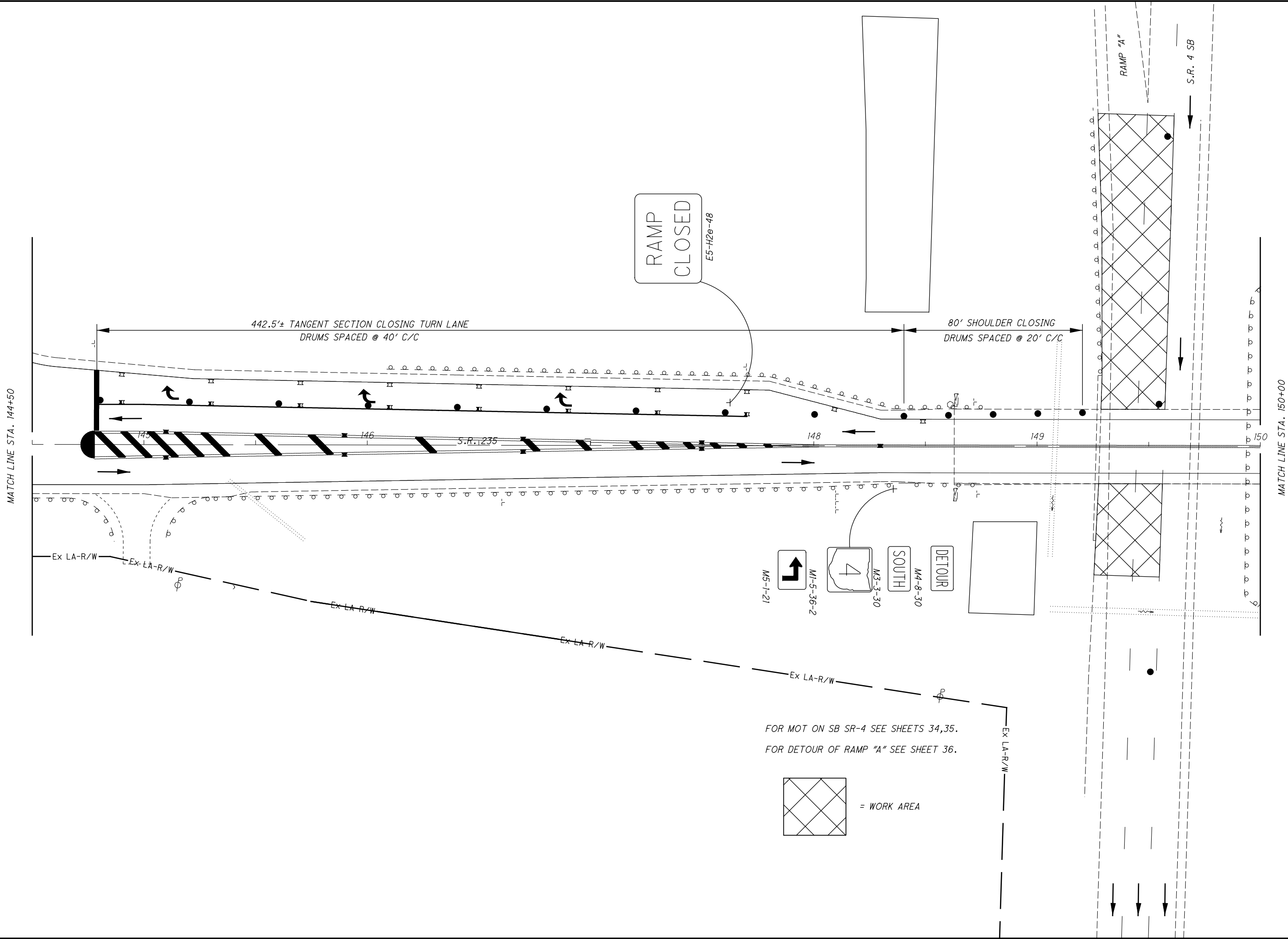
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FOR MOT ON SB SR-4 SEE SHEETS 34,35.  
FOR DETOUR OF RAMP "A" SEE SHEET 36.

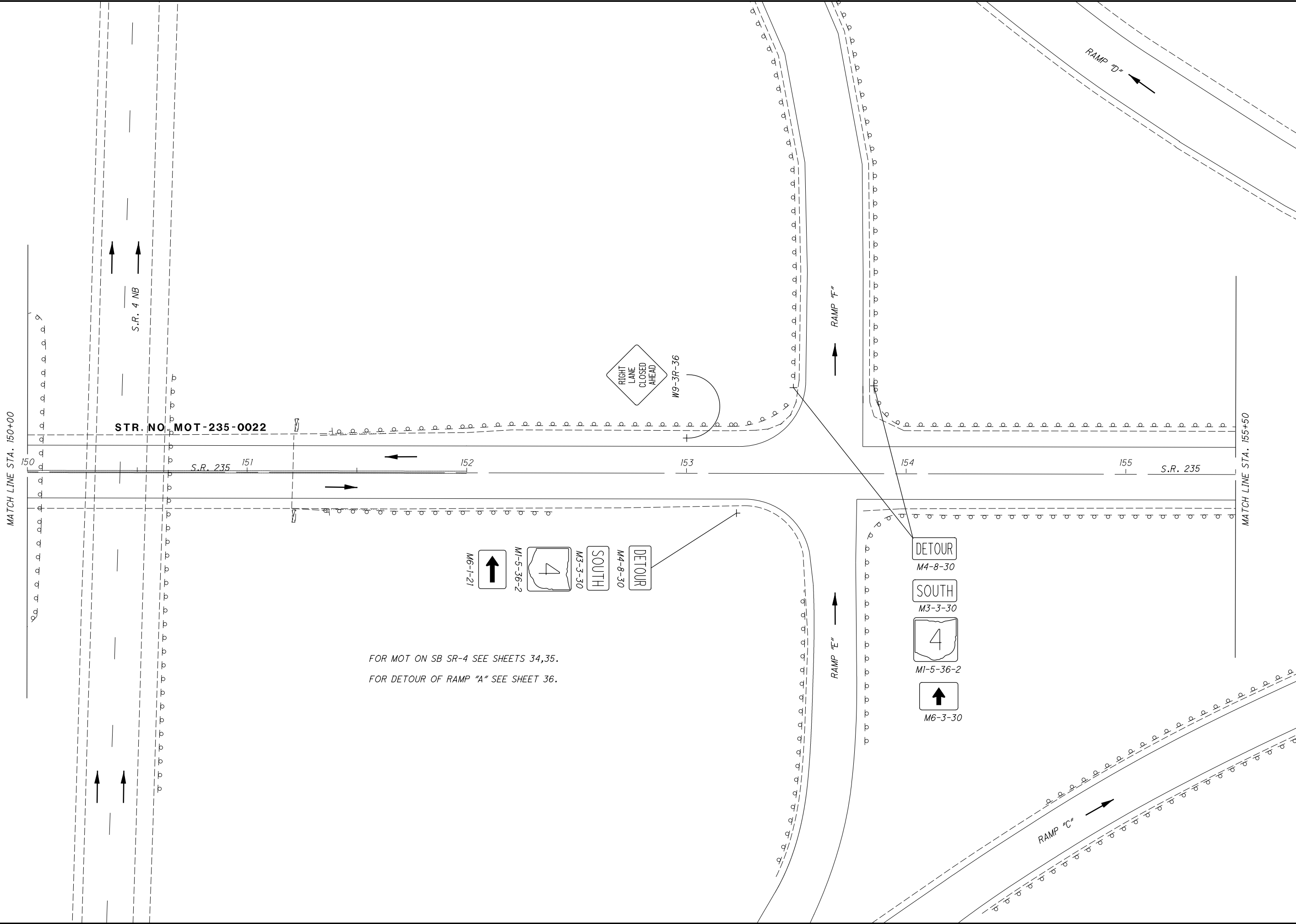


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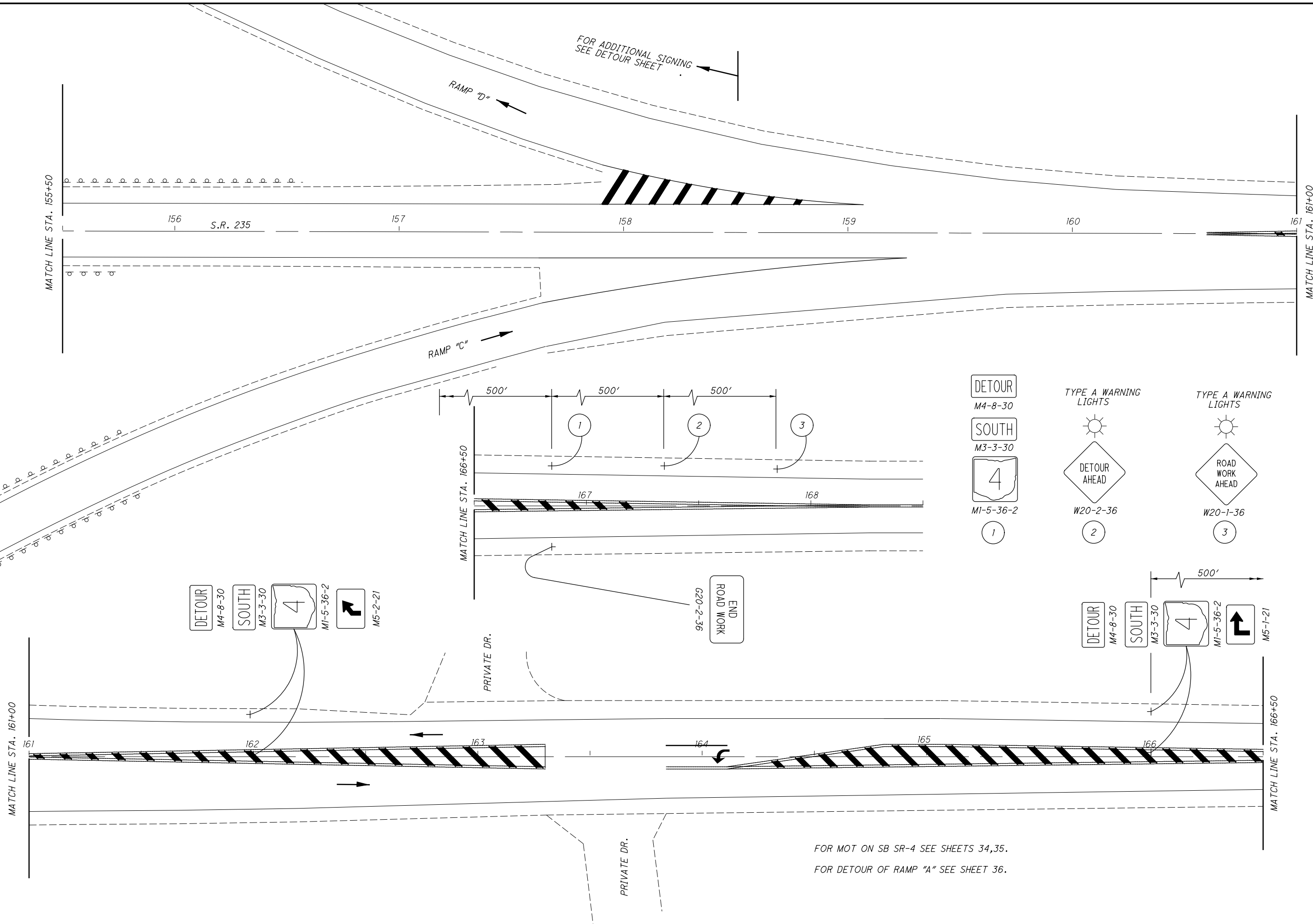
31 67	D07 - BH - FY 20(B)	MAINTENANCE OF TRAFFIC - (3 OF 8) CHAMBERSBURG RD / SR-235 AND SR-4 INTERCHANGE	CALCULATED REB	CHECKED XXX	 0 20 40 HORIZONTAL SCALE IN FEET





32 67	D07 - BH - FY 20(B)	MAINTENANCE OF TRAFFIC - (4 OF 8) CHAMBERSBURG RD / SR-235 AND SR-4 INTERCHANGE			CALCULATED REB CHECKED XXX	 0 10 20 40 HORIZONTAL SCALE IN FEET

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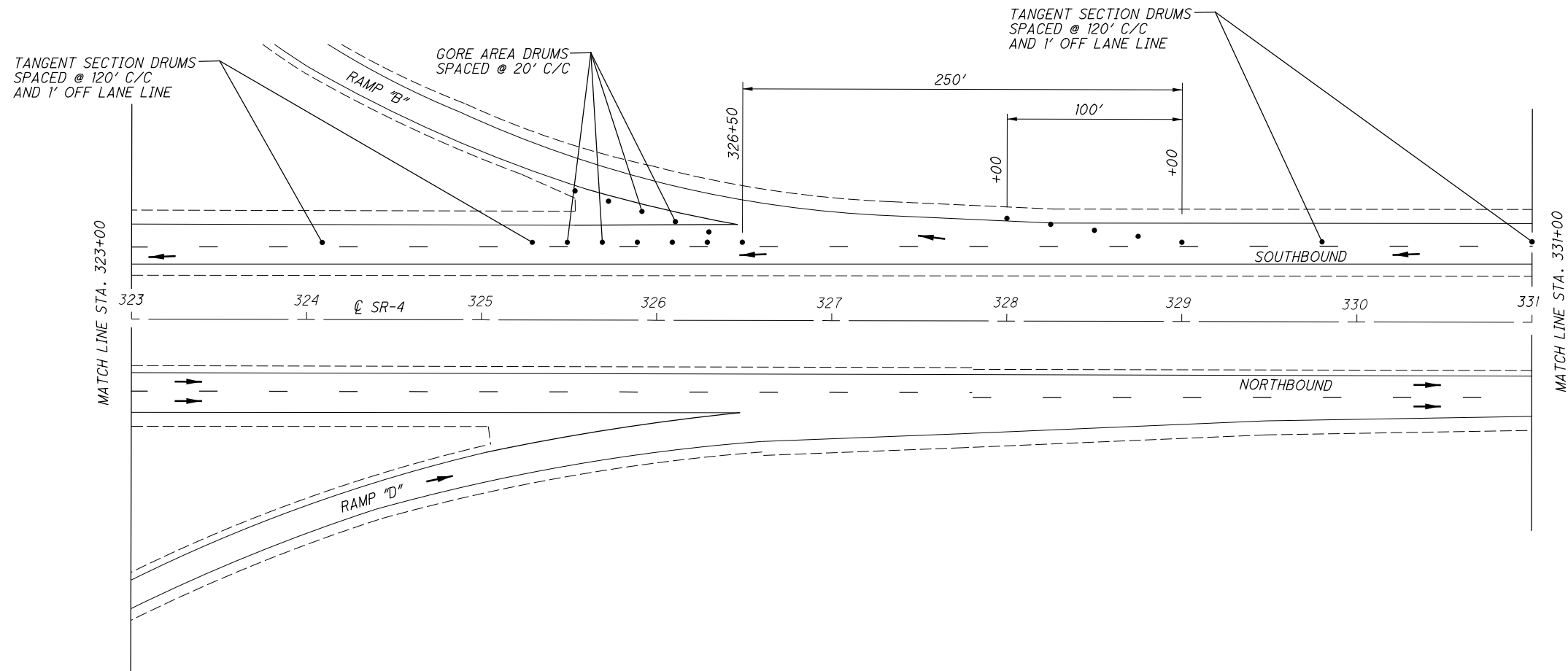
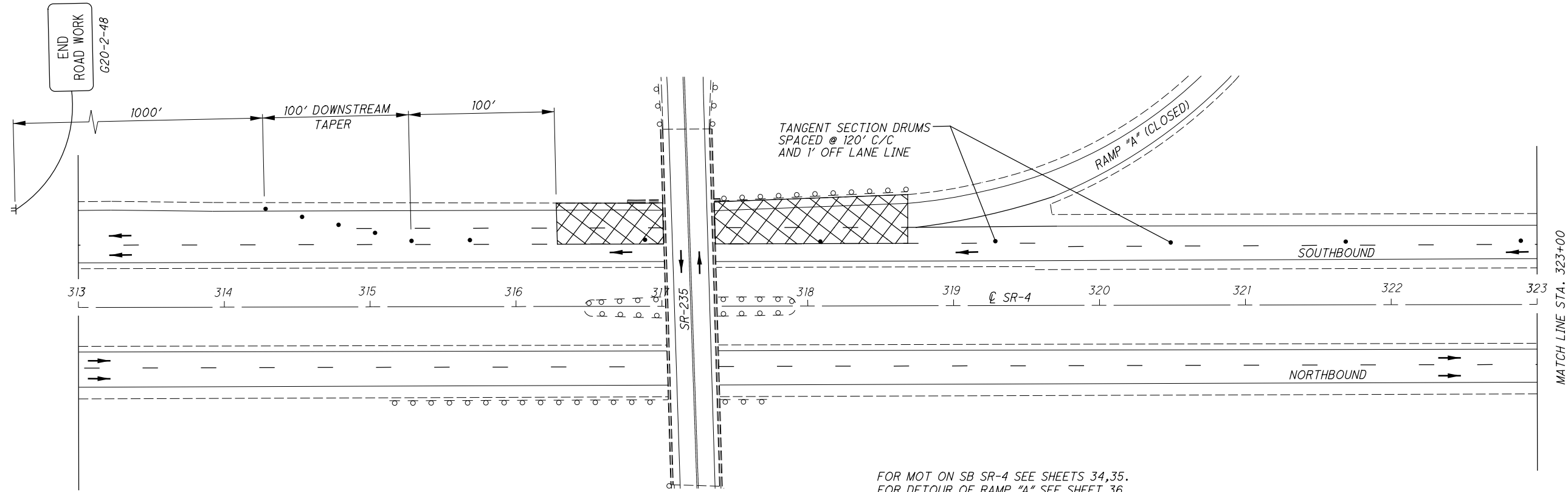
DETOUR M4-8-30	TYPE A WARNING LIGHTS	TYPE A WARNING LIGHTS
SOUTH M3-3-30		
	DETOUR AHEAD	ROAD WORK AHEAD
M1-5-36-2	W20-2-36	W20-1-36
1	2	3

DETOUR M4-8-30	SOUTH M3-3-30		M1-5-36-2		M5-1-21
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500'

FOR MOT ON SB SR-4 SEE SHEETS 34,35.  
FOR DETOUR OF RAMP "A" SEE SHEET 36.

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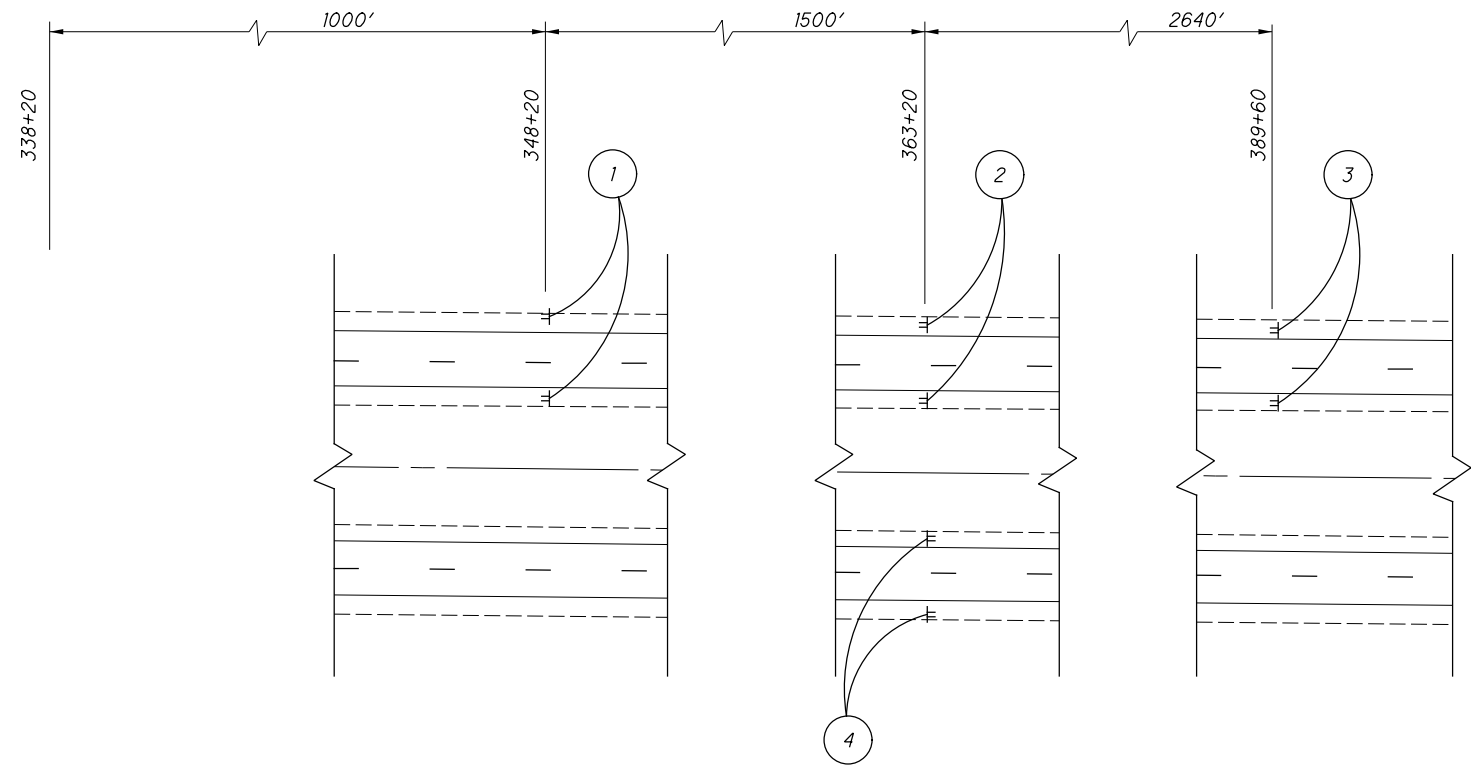
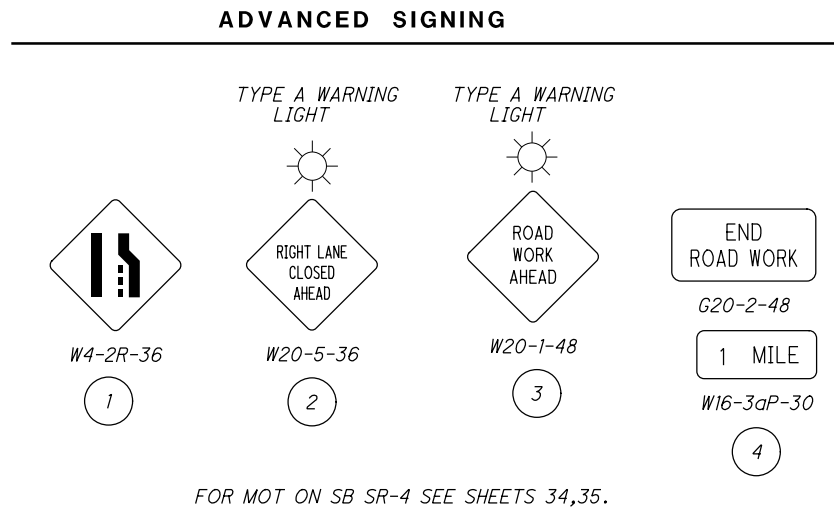
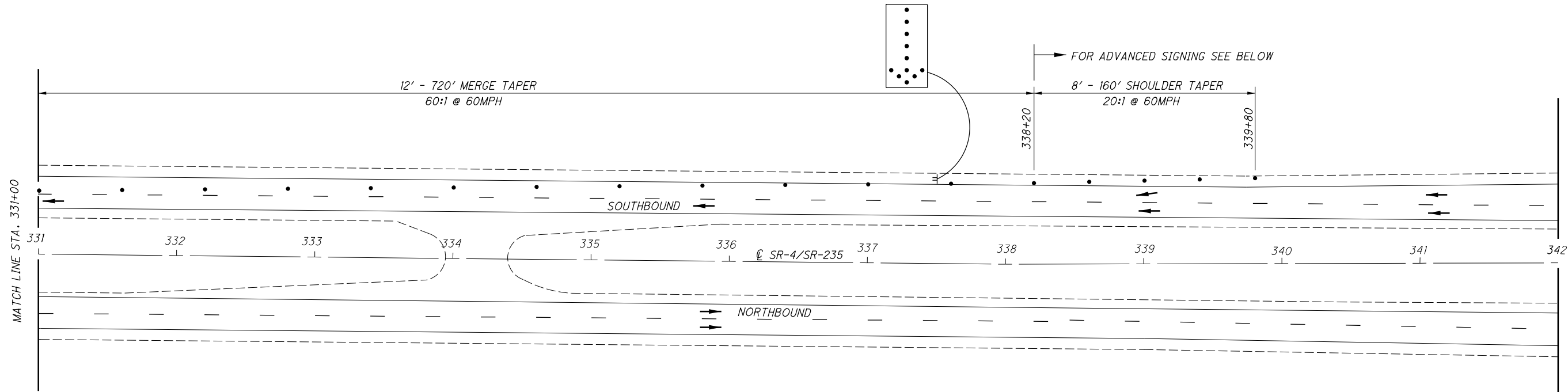


CALCULATED  
REB  
CHECKED  
XXX






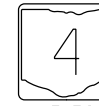





MAINTENANCE OF TRAFFIC - (6 OF 8)  
CHAMBERSBURD RD / SR-235 AND SR-4 INTERCHANGE

D07 - BH - FY 20(B)

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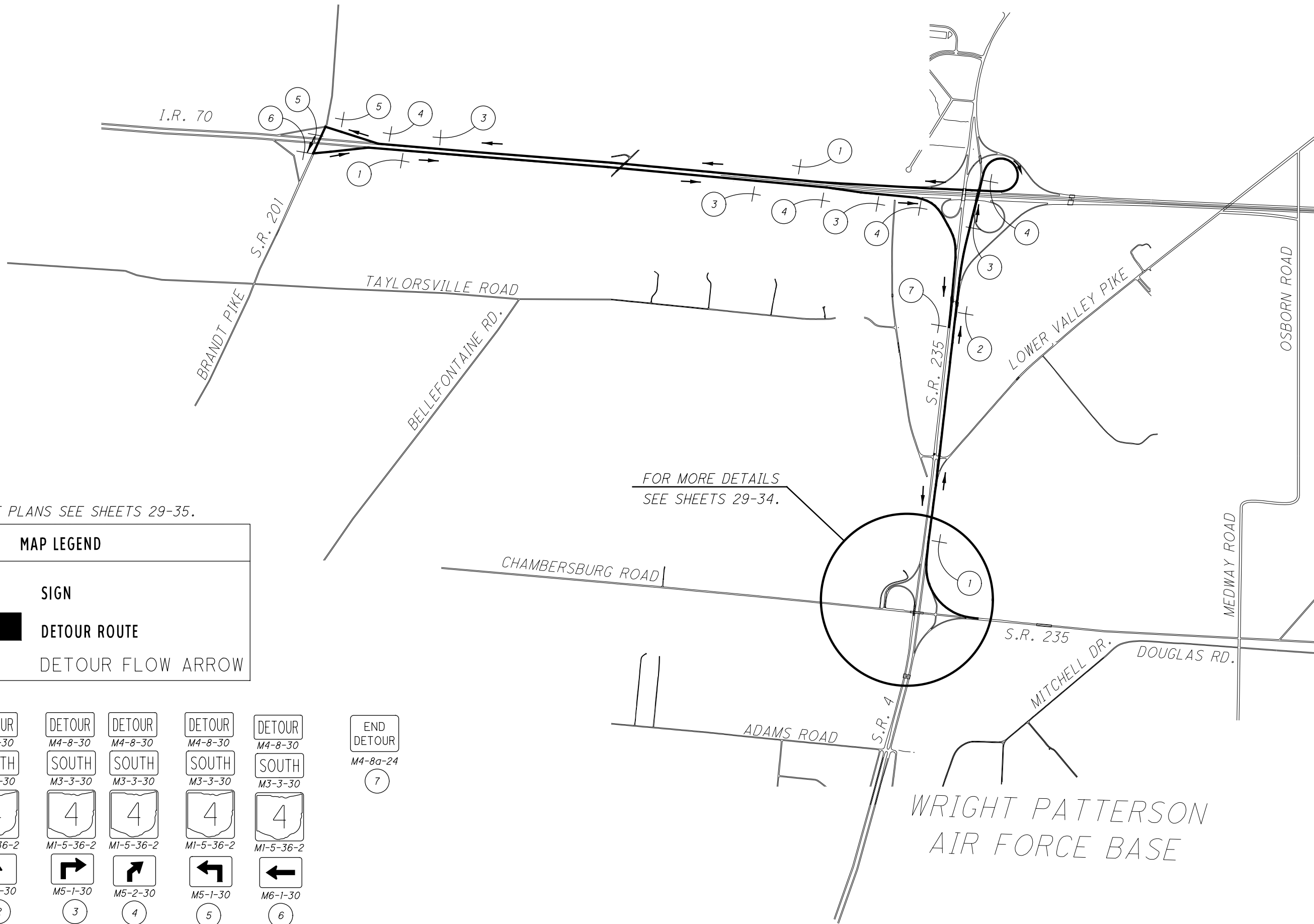
DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30
SOUTH M3-3-30	SOUTH M3-3-30	SOUTH M3-3-30	SOUTH M3-3-30	SOUTH M3-3-30	SOUTH M3-3-30
					
1	 M6-3-30	 M5-1-30	 M5-2-30	 M5-1-30	 M6-1-30
	2	3	4	5	6

END  
DETOUR  
M4-8a-24

7

FOR MOT PLANS SEE SHEETS 29-35.

MAP LEGEND	
	SIGN
	DETOUR ROUTE
	DETOUR FLOW ARROW



CALCULATED REB CHECKED XXX	 0 100000 200000 HORIZONTAL SCALE IN FEET
DETOUR SHEET - (8 OF 8)	
CHAMBERSBURG RD / SR-235 AND SR-4 INTERCHANGE	
D07-BH-FY20(B)	36 67



SHEET NUM.									PART.					ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	CALCULATED REB CHECKED DHG
			6	7	39			Calc's	01/IMS/BR	02/NHS/BR	03/S<2/BR	04/S>2/BR	05/NHS/BR							
																	ROADWAY			
									LS					201	11000	LS	CLEARING AND GRUBBING			
					50				50					202	75201	50	FENCE REMOVED FOR REUSE, AS PER PLAN	39		
					50				50					607	23004	50	FENCE REBUILT, TYPE CLT	4		
																	EROSION CONTROL			
					47				47					601	20000	47	CRUSHED AGGREGATE SLOPE PROTECTION			
									1,330					832	30000	1,330	EROSION CONTROL			
																	DRAINAGE			
					4				4					613	41201	4	LOW STRENGTH MORTAR BACKFILL, AS PER PLAN	39		
																	STRUCTURE REPAIR (AUG-033-1507L)			
					8					8				516	46701	8	RESET BEARING, AS PER PLAN	5		
					LS					LS				516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (AUG-066-1273)			
					12					12				516	46701	12	RESET BEARING, AS PER PLAN	5		
					LS					LS				516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (CLA-004-0611R)			
					13				13					513	10201	13	STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN	5		
					LS				LS					513	95020	LS	STRUCTURAL STEEL, MISC.:REPAIR CRACKED WELDS	5		
					2				2					516	46701	2	RESET BEARING, AS PER PLAN	5		
					LS				LS					516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (CLA-054-0561)			
					10				10					516	46701	10	RESET BEARING, AS PER PLAN	5		
					LS				LS					516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (CLA-070-0173)			
					13				13					516	45305	13	REFURBISH BEARING DEVICE, AS PER PLAN	5		
					LS				LS					516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (CLA-070-1446)			
					7				7					513	10201	7	STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN	5		
																	STRUCTURE REPAIR (CLA-070-2631)			
					3				3					516	46701	3	RESET BEARING, AS PER PLAN	5		
					LS				LS					516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (CLA-072-0863)			
					12						12			516	46701	12	RESET BEARING, AS PER PLAN	5		
					LS						LS			516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (CLA-334-0086)			
					9							9		516	46701	9	RESET BEARING, AS PER PLAN	5		
					LS							LS		516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (CLA-334-0099)			
					6							6		516	46701	6	RESET BEARING, AS PER PLAN	5		
					LS							LS		516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (MER-029-1429L)			
					10						10			516	46701	10	RESET BEARING, AS PER PLAN	5		
					LS						LS			516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (MER-029-1429R)			
					10						10			516	46701	10	RESET BEARING, AS PER PLAN	5		
					LS						LS			516	47001	LS	JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
																	STRUCTURE REPAIR (MIA-055-1183)			
					3,860							3,860		513	10201	3,860	STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN	5		
																	STRUCTURE REPAIR (MOT-004-2050L)			
					LS					LS				202	11203	LS	PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	5		
					125					125				516	01300	125	ELASTOMERIC STRIP SEAL WITHOUT STEEL EXTRUSIONS			
					17					17				519	12300	17	PATCHING CONCRETE BRIDGE DECK - TYPE B			
																			37	
																			67	

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SHEET NUM.									PART.					ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	CALCULATED	REB CHECKED	DHG
			6	7	39			Calc's	01/IMS/BR	02/NHS/BR	03/S<2/BR	04/S>2/BR	05/NHS/BR									
					LS					LS				202	11203	LS			STRUCTURE REPAIR (MOT-035-1395L)			
					116					116				516	01300	116	FT		PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	5		
																			ELASTOMERIC STRIP SEAL WITHOUT STEEL EXTRUSIONS			
					LS					LS				202	11203	LS			STRUCTURE REPAIR (MOT-035-1395R)			
					135					135				516	01300	135	FT		PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	5		
					9					9				519	12300	9	SY		ELASTOMERIC STRIP SEAL WITHOUT STEEL EXTRUSIONS			
																			PATCHING CONCRETE BRIDGE DECK - TYPE B			
					2					2				516	46701	2	EACH		STRUCTURE REPAIR (MOT-035-1629R)			
					LS					LS				516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
					1					1				516	46701	1	EACH		STRUCTURE REPAIR (MOT-048-1214)			
					LS					LS				516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
					1				1					516	46701	1	EACH		STRUCTURE REPAIR (MOT-049-1091L)			
					LS				LS					516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
					LS				LS					513	95020	LS			STRUCTURE REPAIR (MOT-075-0373L)			
																			STRUCTURAL STEEL, MISC.:REPAIR CRACKED WELDS	5		
					8				8					516	46701	8	EACH		STRUCTURE REPAIR (MOT-075-1466)			
					LS				LS					516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
					40				40					516	46201	40	EACH		STRUCTURE REPAIR (MOT-075-1503)			
					12				12					516	46701	12	EACH		BEARING DEVICE, ROCKER, AS PER PLAN	5		
					LS				LS					516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
					LS								LS	202	11203	LS			STRUCTURE REPAIR (MOT-235-0022L)			
					298									513	10201	298	LB		PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	5		
																			STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN	5		
					LS				LS					202	11203	LS			STRUCTURE REPAIR (MOT-675-0063R)			
					104				104					513	10201	104	LB		PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	5		
					6				6					516	46701	6	EACH		STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN	5		
					LS				LS					516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
					18				18					516	46701	18	EACH		STRUCTURE REPAIR (MOT-675-0129)			
					LS				LS					516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
					2				2					516	46701	2	EACH		STRUCTURE REPAIR (MOT-675-0233)			
					LS				LS					516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
					12				12					516	46701	12	EACH		STRUCTURE REPAIR (MOT-675-0647)			
					LS				LS					516	47001	LS			RESET BEARING, AS PER PLAN	5		
																			JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	5		
				100										614	11111	100	HOURL		MAINTENANCE OF TRAFFIC			
									LS					614	12420	LS			LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE, AS PER PLAN	7		
			5											614	12500	5	EACH		DETOUR SIGNING			
			10											614	12600	10	EACH		REPLACEMENT SIGN			
				6										614	18601	6	SNMT		REPLACEMENT DRUM			
																			PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	7		
														614	11000	LS			INCIDENTALS			
														623	10000	LS			MAINTAINING TRAFFIC			
														624	10000	LS			CONSTRUCTION LAYOUT STAKES AND SURVEYING			
																			MOBILIZATION			

39  
67

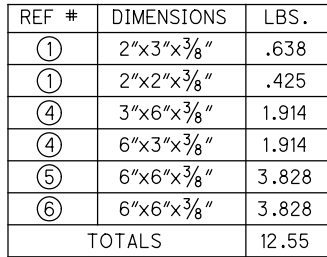


6). SEE SHEET 5 FOR ADDITIONAL NOTES.



1). RESET BEARINGS, 1,3,4,5,6,7,8,10 @ REAR ABUTMENT  
2). RESET BEARINGS, 1,3,,6,7 @ FORWARD ABUTMENT





## EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL GIRDERS WITH REINFORCED  
CONCRETE DECK AND SUB STRUCTURE  
SPANS: 78'-0", 120'-0", 120'-0", 78'-0" c/c BEARINGS  
ROADWAY: 30'-0" TOE TO TOE BARRIER  
LOADING: CF 2000 (57) ADEQUATE FOR  
AASHTO ALTERNATE LOADING  
SKEW: 49°-52'-00" RT. FWD.  
APPROACH SLABS: AS-1-54 (25" LONG)  
ALIGNMENT: TANGENT  
CROWN: 3/16" /FT  
STRUCTURAL FILE NUMBER: 1200011  
DATE BUILT: 1967  
DISPOSITION: TO BE REPAIRED

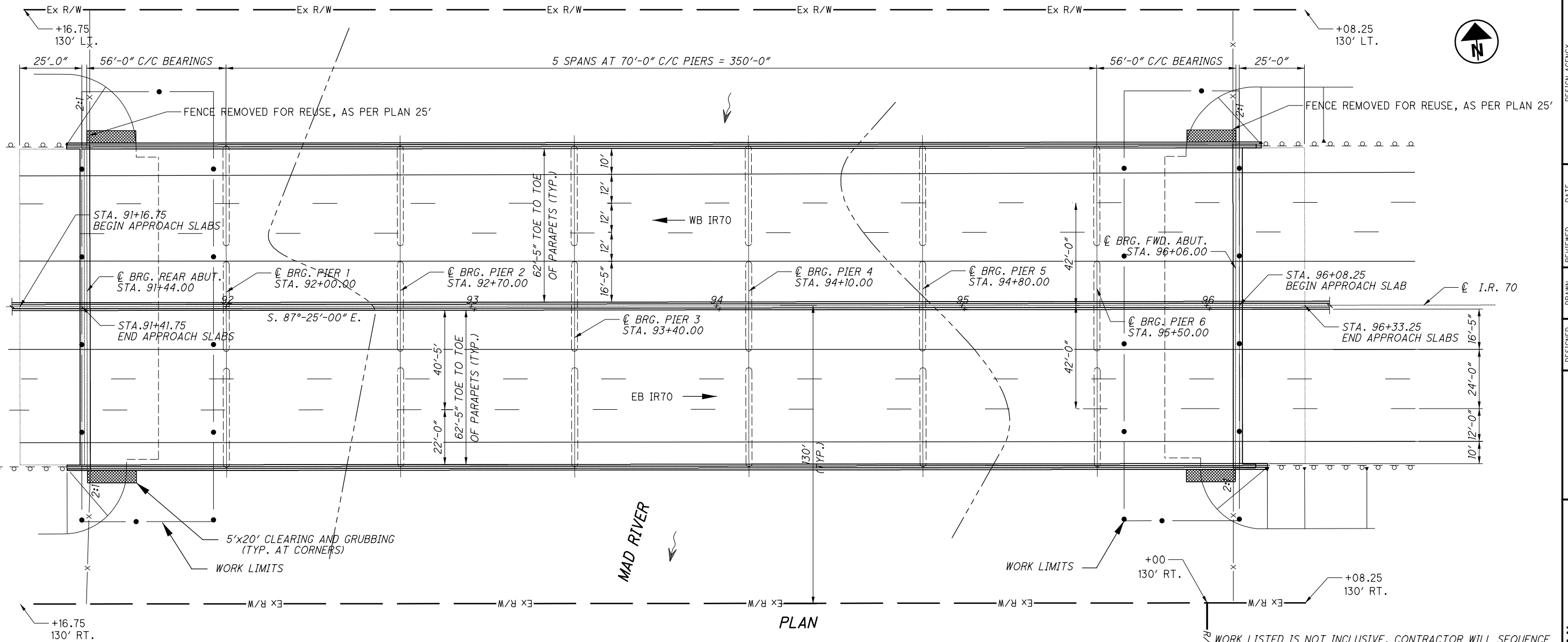
### PROPOSED WORK

- 1). REPAIR SECTION LOSS AT BEAM 1 AND STIFFENER AT REAR ABUTMENT.
- 2). RESET BEARING 3 AT REAR ABUTMENT.
- 3). REPAIR END FRAME ANGLE WELDS BETWEEN BEAM 3 & 4, REAR ABUTMENT.
- 4). REPAIR SECTION LOSS AT BEAM 4 AND STIFFENER AT REAR ABUTMENT.
- 5). REPAIR SECTION LOSS AT STIFFENER, BEAM 5 AT REAR ABUTMENT.
- 6). REPAIR SECTION LOSS AT STIFFENER, BEAM 1 AND RESET BEARING 1 AT FWD. ABUTMENT.

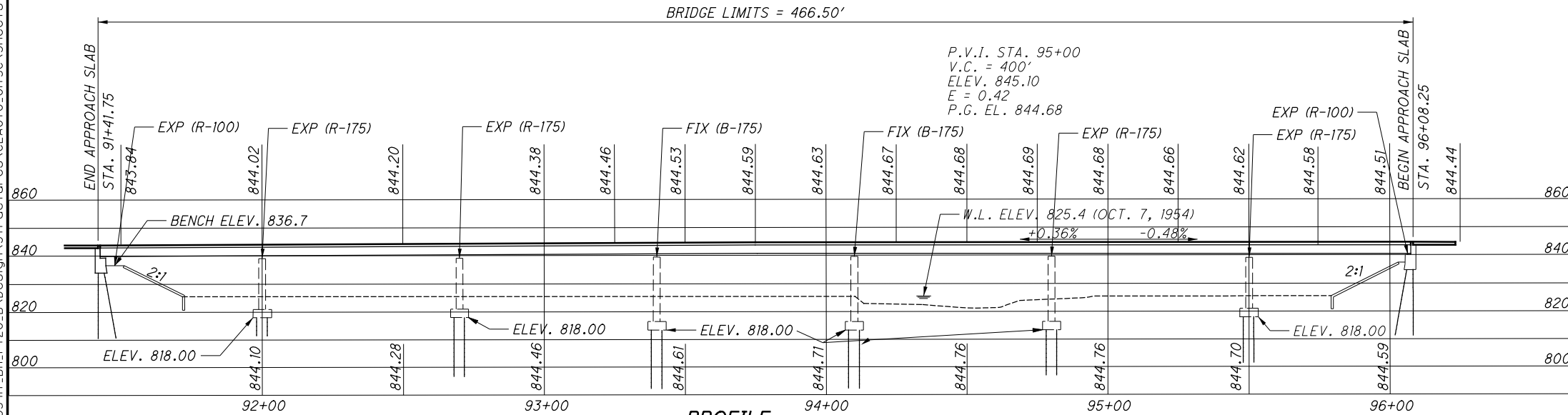




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PLAN



PROFILE

NOTE:

- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
- 2). BEARINGS ARE NUMBERED 1-18, LEFT TO RIGHT WHEN LOOKING UPSTATION ON IR-70.
- 3). ABUTMENT BEARINGS ARE R-100

NOTE: (con't.)

- 4). STANDARD DRAWING RB-1-55 DATED 2-2-59
- 5). SEE SHEET 5 FOR ADDITIONAL NOTES.

5'x20' CLEARING AND GRUBBING

EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL BEAM (A36) WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: 56'-0", 5 @ 70'-0", 56'-0" C/C BEARINGS  
ROADWAY: 2 @ 62'-5" TOE TO TOE OF BARRIERS  
LOADING: HS20-44 CASE 1 AND THE ALTERNATE MILITARY LOADING  
SKEW: NONE  
APPROACH SLABS: AS-1-81 (25'-0" LONG)  
ALIGNMENT: TANGENT  
CROWN: 3/16" /FT  
STRUCTURAL FILE NUMBER: 1203630  
DATE BUILT: 1957  
DISPOSITION: TO BE REPAIRED

PROPOSED WORK

- 1). REFURBISH REAR ABUTMENT BEARINGS 1-6.
- 2). REFURBISH FORWARD ABUTMENT BEARINGS 1,3,5-9.
- 3). FENCE REMOVED FOR REUSE, AS PER PLAN - 50'
- 3). FENCE REBUILT, TYPE CLT - 50'

DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

REVIEWED  
MRB  
DATE  
10/1/19

DRAWN  
PUB  
REVIS  
XXX

DESIGNED  
REB  
CHECKED  
DHG

CLARK COUNTY  
STA. 91+16.75  
STA. 96+33.25

SITE PLAN  
BRIDGE NO. : CLAO-070-0173  
I.R. 70 OVER MAD RIVER

D07 BH FY20(B)  
PID No. 105417

1 / 1

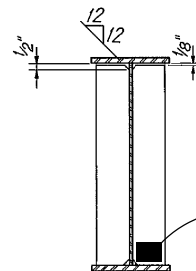
44  
67



- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED  $\pm$  AND FOR REFERENCE ONLY.
- 2). BEARINGS ARE NUMBERED 1-5 LEFT TO RIGHT WHEN LOOKING UP STATION OF SELMA PIKE.
- 3). ABUTMENT BEARINGS ARE E-100.
- 4). STANDARD DRAWING FBS-1-62
- 5). SEE SHEET 5 FOR ADDITIONAL NOTES.
- 6). PLATE SIZES ARE INFORMATION ONLY, COULD VARY IN THE FIELD.
- 7). BALLOONS COINCIDE WITH PROPOSED WORK.

REQ'D. MIN. HORIZ.  
CL. = 16.00'

TOTALS	6.56
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1) 3"x6"x $\frac{5}{16}$ " PLATE

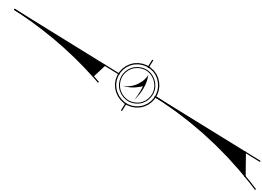


## PROFILE

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

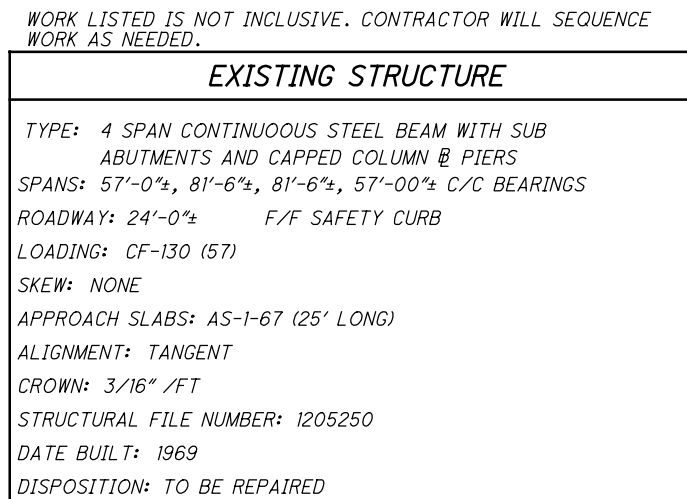
DISPOSITION: TO BE REPAIRED

- 1). REPAIR SECTION LOSS OF STIFFENER, BEAM 5, REAR ABUTMENTS.
- 2). REPAIR SECTION LOSS OF BEAM 5, FORWARD ABUTMENTS.



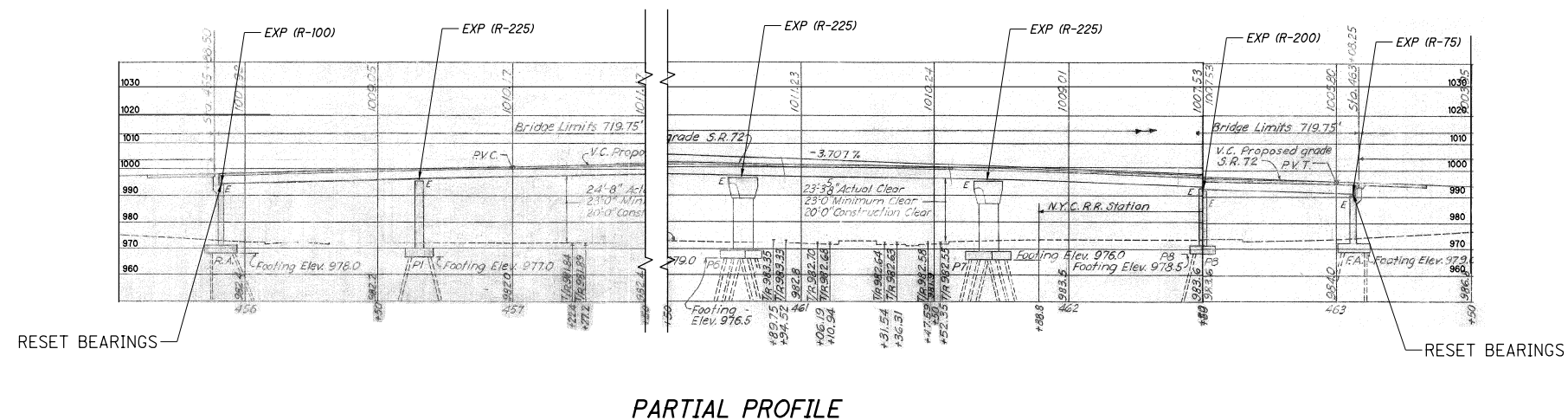
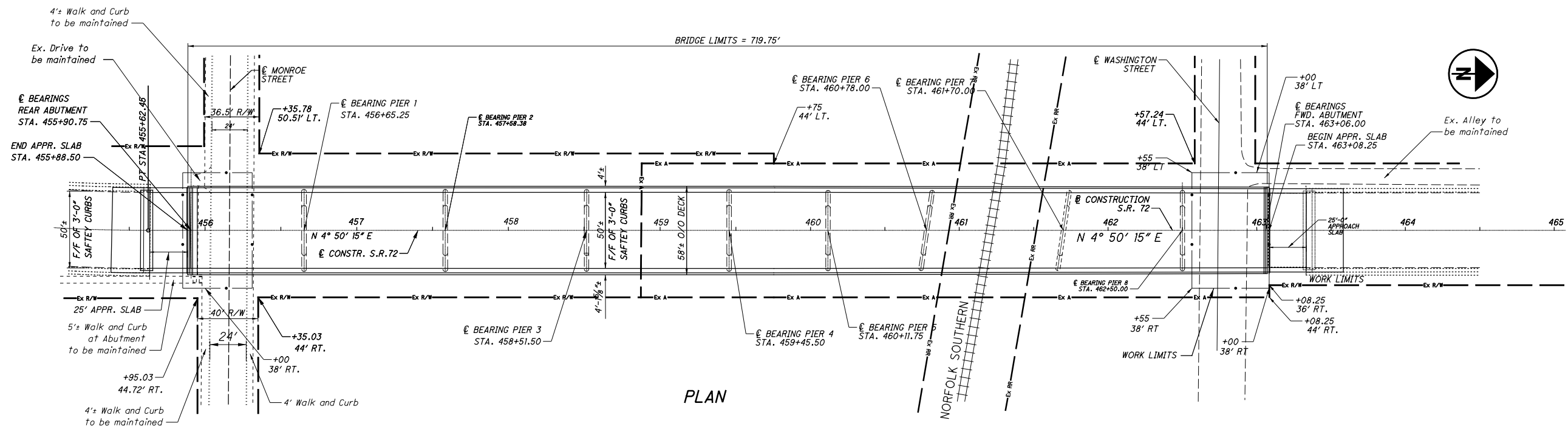
NOTE:

- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED  $\pm$  AND FOR REFERENCE ONLY.
- 2). BEARINGS ARE NUMBERED 1-4 LEFT TO RIGHT WHEN LOOKING UPSTATION OF SYLVAN SHORES RD.
- 3). ABUTMENT BEARINGS ARE R-75.
- 4). STANDARD DRAWING RB-1-55 DATED 2-2-59 (REVISED)
- 5). SEE SHEET 5 FOR ADDITIONAL NOTES.
- 6). R/W FOR SYLVAN SHORES ROAD IS OUTSIDE THIS VIEW.



### PROPOSED WORK

- 1). RESET BEARING #1 AT REAR ABUTMENT.
- 2). RESET BEARINGS #1,#4 AT FORWARD ABUTMENT.



- NOTE:
- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED  $\pm$  AND FOR REFERENCE ONLY.
  - 2). BEARINGS ARE NUMBERED LEFT TO RIGHT 1-7, WHEN LOOKING UPSTREAM ON SR 72.
  - 3). BEARINGS ARE R-100 AT REAR ABUTMENT, AND R-75 AT FORWARD ABUTMENT.
  - 4). STANDARD DRAWING RB-1-55 DATED 2-2-59
  - 5). SEE SHEET 5 FOR ADDITIONAL NOTES.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

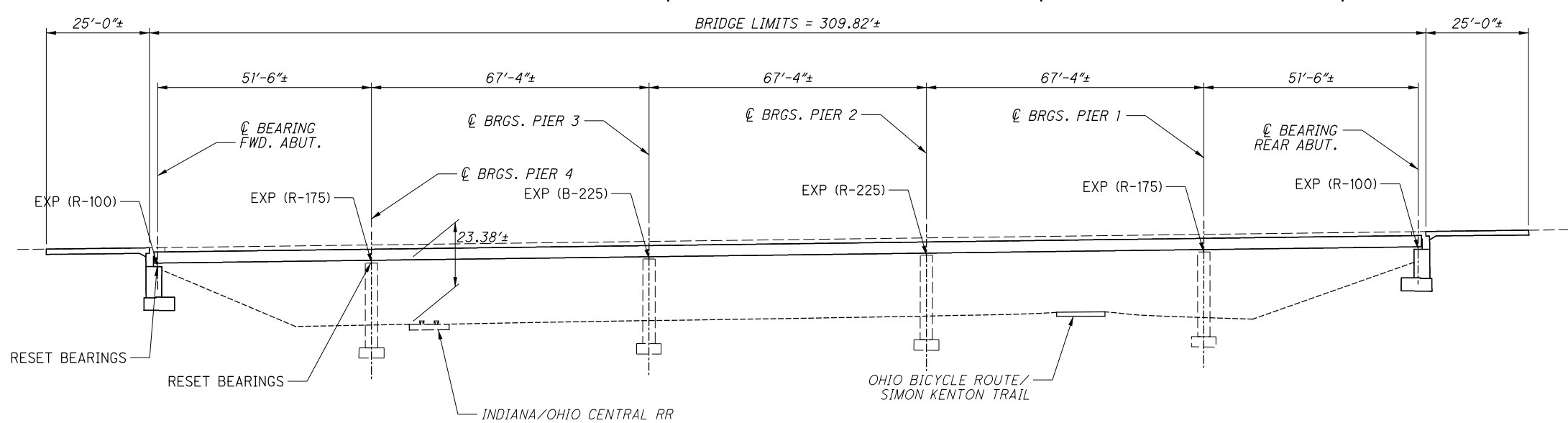
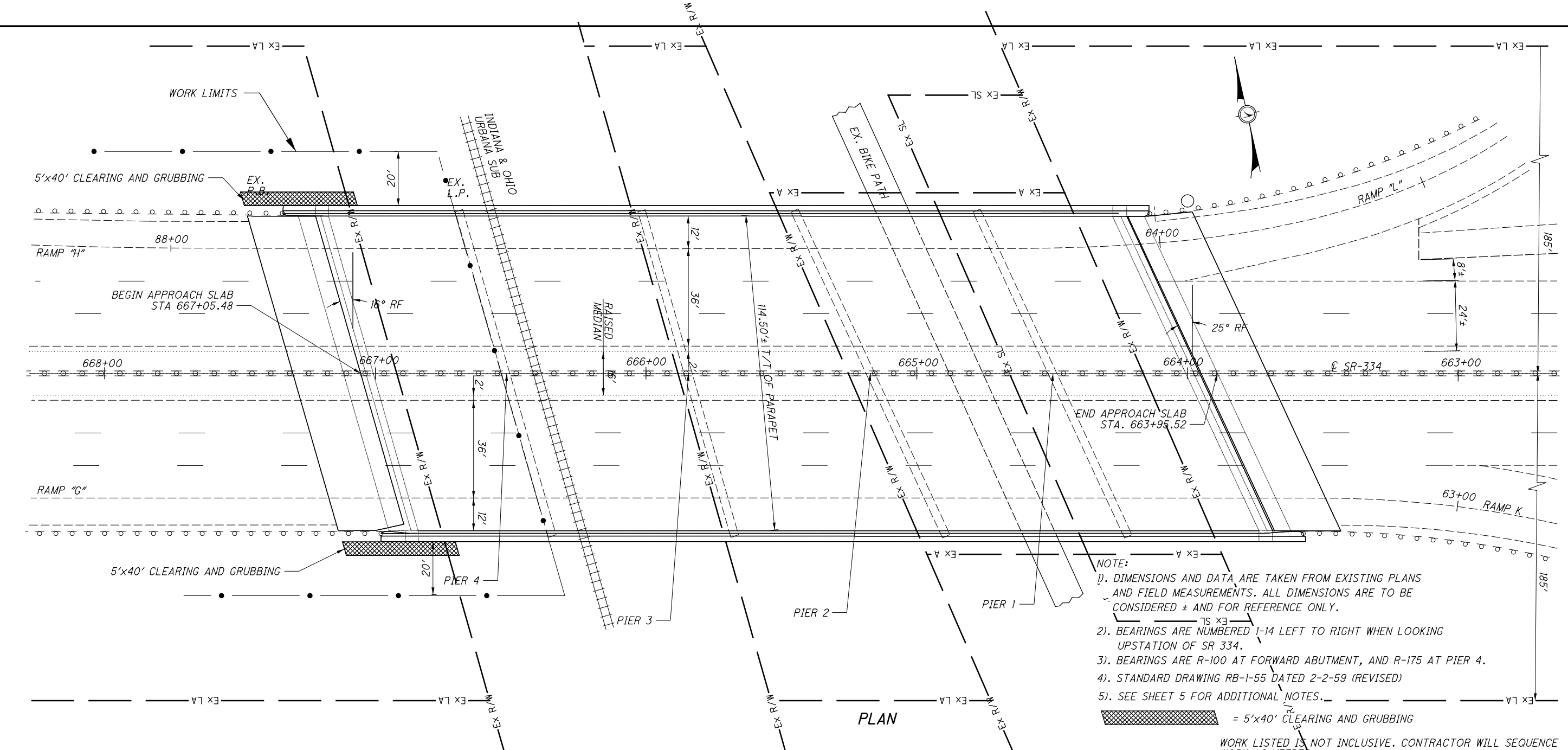
### EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL BEAM WITH REINFORCED  
CONCRETE DECK AND SUBSTRUCTURE  
SPANS: 74'-6"; 93'-1 1/2"; 93'-1 1/2"; 94'-0"  
66'-3"; 66'-3"; 92'-0"; 80'-0"; 56'-0"  
ROADWAY: 50'-0" F/F OF 3'-0" SAFETY CURBS  
LOADING: HS20, CASE II, AND THE ALTERNATE MILITARY  
SKEW: 0°0'0" AND 8°28'40" L.F.  
APPROACH SLABS: AS-1-81 (25' LONG)  
ALIGNMENT: TANGENT  
CROWN: 3/16" /FT  
STRUCTURAL FILE NUMBER: 1205463  
DATE BUILT: 1970  
DISPOSITION: TO BE REPAIRED

### PROPOSED WORK

- 1). RESET ABUTMENT BEARINGS #1, #2, #5-#7 REAR
- 2). RESET ABUTMENT BEARINGS #1-#7 FORWARD

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PROFILE

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

### EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL BEAMS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: VARIABLE 51'-6"; (3) @ 67'-4"; 51'-6" ON  $\angle$  SURVEY

ROADWAY: 116'-0" T/T OF CONCRETE PARAPETS MEDIAN W/ 16'-0" WIDTH RAISED

LOADING: HS 20-44 & ALTERNATE MILITARY

SKEW: VARIABLE 25°RF (R.ABUT.) AND 16°RF (F.ABUT.)

APPROACH SLABS: AS-1-67 (25' MODIFIED)

ALIGNMENT: TANGENT

CROWN: 0.016

STRUCTURAL FILE NUMBER: 1205641

DATE BUILT: 1971

DISPOSITION: TO BE REPAIRED

### PROPOSED WORK

- 1). RESET BEARING #5 AT FORWARD ABUTMENT.
- 2). RESET BEARINGS #1-#5 AND #8-#10 AT PIER 4 THAT ARE OUT OF ADJUSTMENT.

BRIDGE NO. : CL334-0086  
S.R.334 OVER BIKE PATH AND INDIANA & OHIO URBANA SUB RR

D07 BH FY20(B)  
PID No. 105417

DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
11/07/19

REVIEWED  
MRB

CLARK COUNTY  
STA. 663+95.52  
STA. 667+05.48

SITE PLAN

DESIGNED  
REB

CHECKED  
DHG

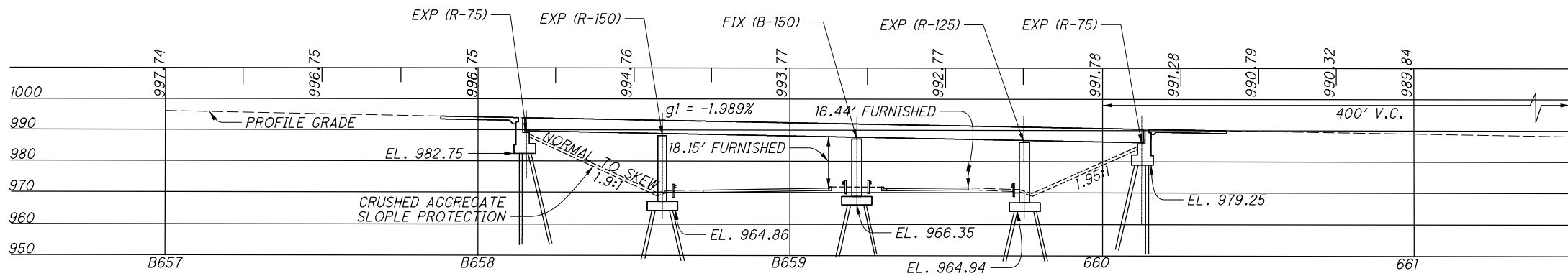
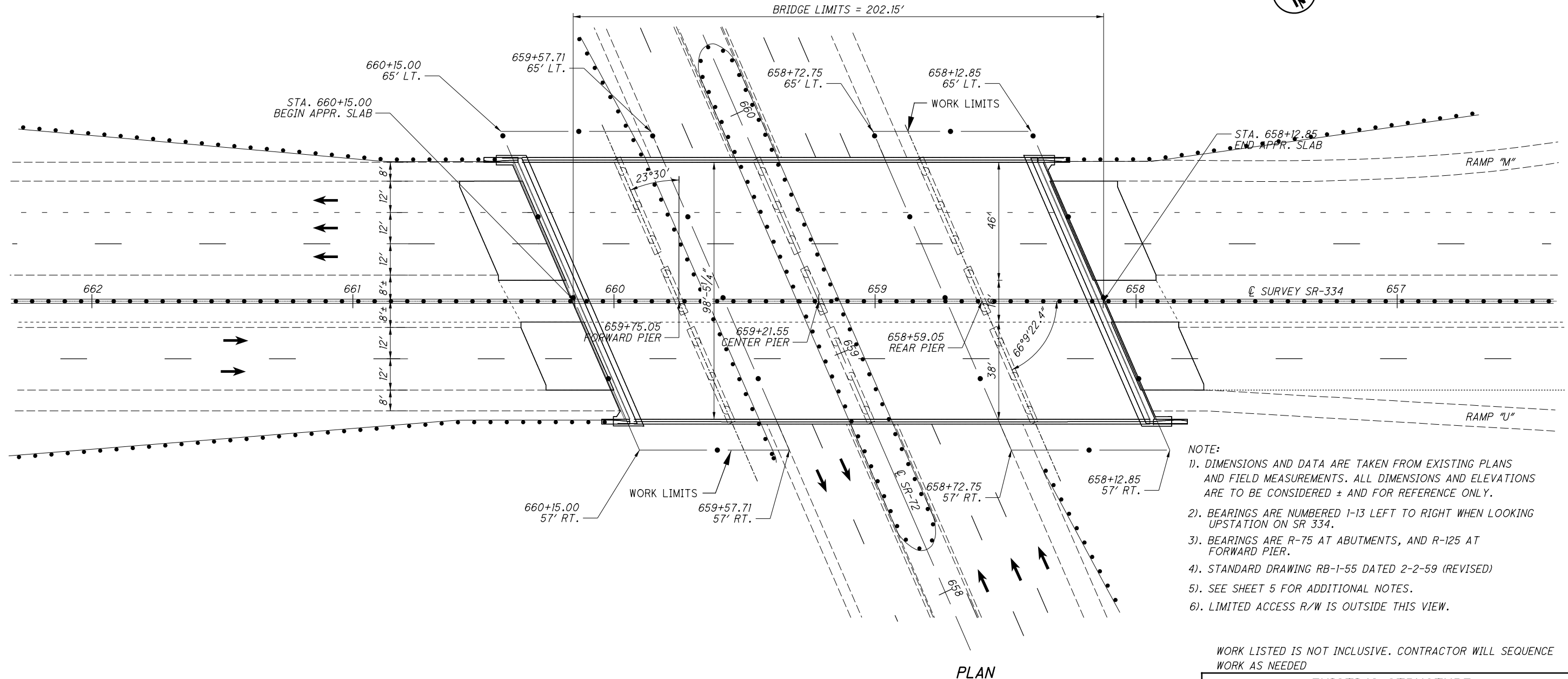
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REVIS  
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48  
67

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WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED

#### EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL BEAM WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: 43'-9", 62'-6", 53'-6", 37'-6" C/C BEARINGS

ROADWAY: 98'-6" T/T PARAPET (16' CONC. MEDIAN)

LOADING: CF 2000 (57)

SKEW: 23°-30'-00" RT. FORWARD

APPROACH SLABS: AS-1-67 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 3/16" /FT

STRUCTURAL FILE NUMBER: 1205617

DATE BUILT: 1971

DISPOSITION: TO BE REPAIRED

#### PROPOSED WORK

- 1). RESET BEARING #8 AT FORWARD ABUTMENT
- 2). RESET BEARINGS #3, #5, #6, AT REAR ABUTMENT
- 3). RESET BEARING #9, #13 AT REAR PIER

DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
10/01/19

REVIEWED  
MRB

STRUCTURE FILE NUMBER  
1205617

DRAWN  
PUB

DESIGNED  
REB

CHECKED  
DHG

REVISED  
XXX

CLARK COUNTY  
STA. 658+12.85  
STA. 660+15.00

#### SITE PLAN

BRIDGE NO. : CL334-0099  
S.R. 334 OVER S.R. 72

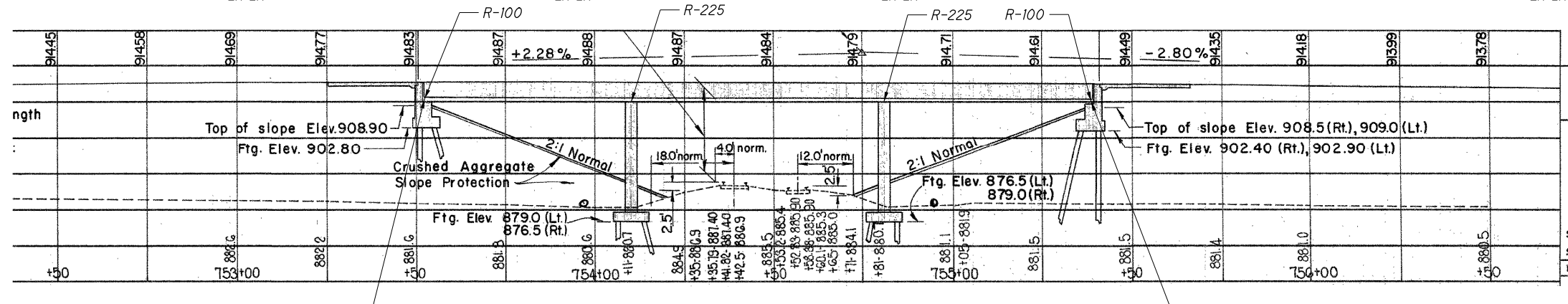
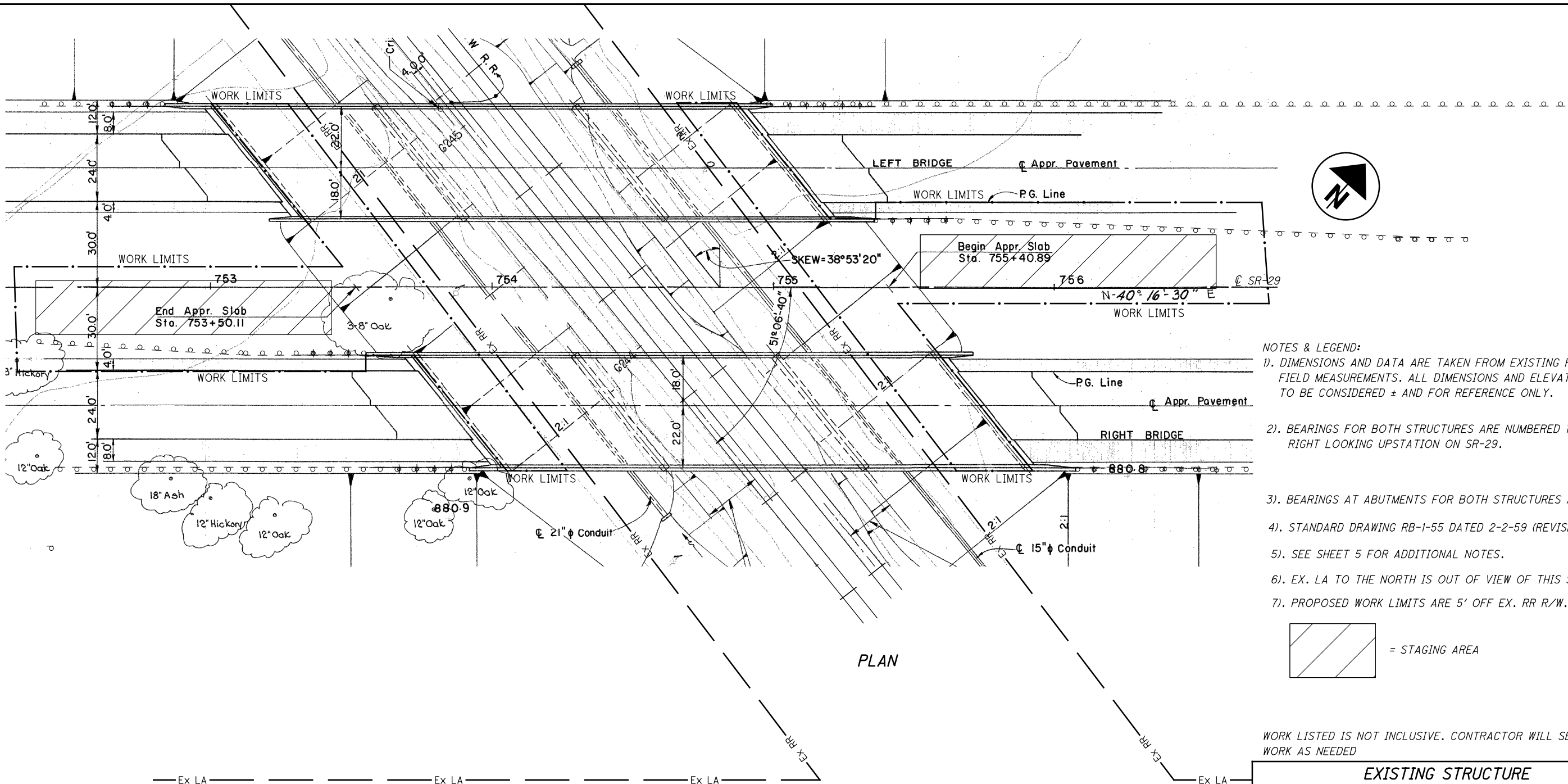
D07 BH FY20(B)  
PID No. 105417

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67



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- NOTES & LEGEND:
- DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
  - BEARINGS FOR BOTH STRUCTURES ARE NUMBERED 1-5 LEFT TO RIGHT LOOKING UPSTATION ON SR-29.
  - BEARINGS AT ABUTMENTS FOR BOTH STRUCTURES ARE R-100.
  - STANDARD DRAWING RB-1-55 DATED 2-2-59 (REVISED)
  - SEE SHEET 5 FOR ADDITIONAL NOTES.
  - EX. LA TO THE NORTH IS OUT OF VIEW OF THIS SHEET.
  - PROPOSED WORK LIMITS ARE 5' OFF EX. RR R/W.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED

**EXISTING STRUCTURE**

TYPE: CONTINUOUS STEEL BEAM WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: 57'-0", 71'-0", 57'-0" C/C BEARINGS

ROADWAY: 2 AT 40'-0" F/F PARAPETS

LOADING: HS 20-44

SKEW: 38°53'-20" RIGHT FORWARD

APPROACH SLABS: AS-1-67 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 3/16" /FT

STRUCTURAL FILE NUMBER: 5400341/5400325

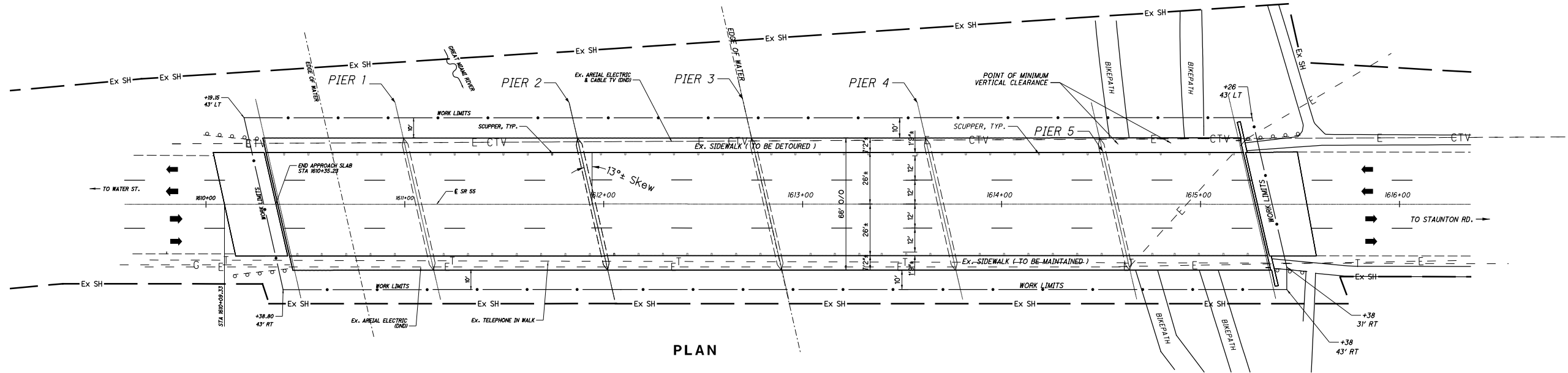
DATE BUILT: 7/1/1971

DISPOSITION: TO BE REPAIRED

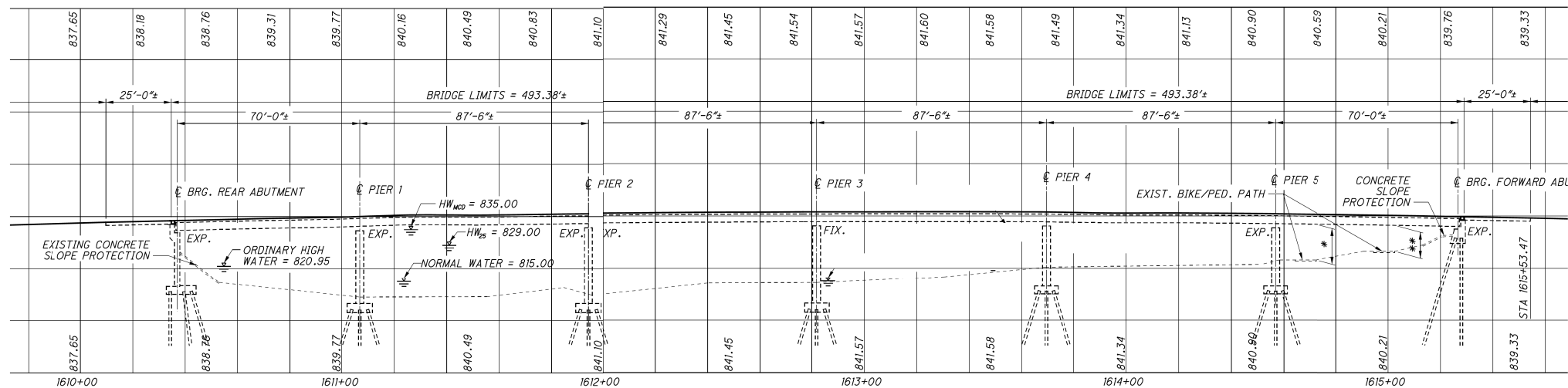
**PROPOSED WORK**

- RESET BEARINGS #1-#5 REAR ABUTMENT LEFT STRUCTURE
- RESET BEARINGS #1-#5 FWD ABUTMENT LEFT STRUCTURE
- RESET BEARINGS #1-#5 REAR ABUTMENT RIGHT STRUCTURE
- RESET BEARINGS #1-#5 FWD ABUTMENT RIGHT STRUCTURE

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- NOTE:
- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENT. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
  - 2). BEAMS ARE NUMBERED 1-10 LEFT TO RIGHT LOOKING UPSTATION OF SR-55.
  - 3). ALL BEAMS ARE 36-WF-230.
  - 4). STANDARD DRAWINGS CSB-2-56 DATED 2-2-59 (REVISED)
  - 5). STANDARD DRAWINGS RB-1-55 DATED 2-2-59 (REVISED)
  - 6). SEE SHEET 5 FOR ADDITIONAL NOTES.
  - 7). REPAIR WORK AT PIERS 2-4 TO BE ACCESSED USING EQUIPMENT STATIONED ON TOP OF THE BRIDGE DECK THAT CAN REACH UNDER THE DECK FROM OVER THE EDGE.



WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

### EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL BEAM WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: 70'± - 87.5'± - 87.5'±  
- 87.5'± - 87.5'± - 70'±

ROADWAY: 52'-0"± F/F CURB

LOADING: HS-20

SKEW: 13° 00' 00" RF

APPROACH SLABS: AS-1-54 (25'± LONG)

ALIGNMENT: TANGENT

CROWN: 3/16"/FT

STRUCTURAL FILE NUMBER: 5501504

DATE BUILT: 1961

DISPOSITION: TO BE REPAIRED

### PROPOSED WORK

- 1). REPAIR BEAM FLANGE SECTION LOSS BEAM #5 AT PIERS #2,#3,#4
- 2). REPAIR BEAM FLANGE SECTION LOSS BEAM #6 AT PIERS #2,#3,#4



DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
11/15/19  
REVIEWED  
MRB  
STRUCTURE FILE NUMBER  
5501504

DRAWN  
PUB  
DESIGNED  
REB  
CHECKED  
DHG  
REVISED  
XXX

MIAMI COUNTY  
STA. 1610+35.23  
STA. 1615+28.61

### SITE PLAN

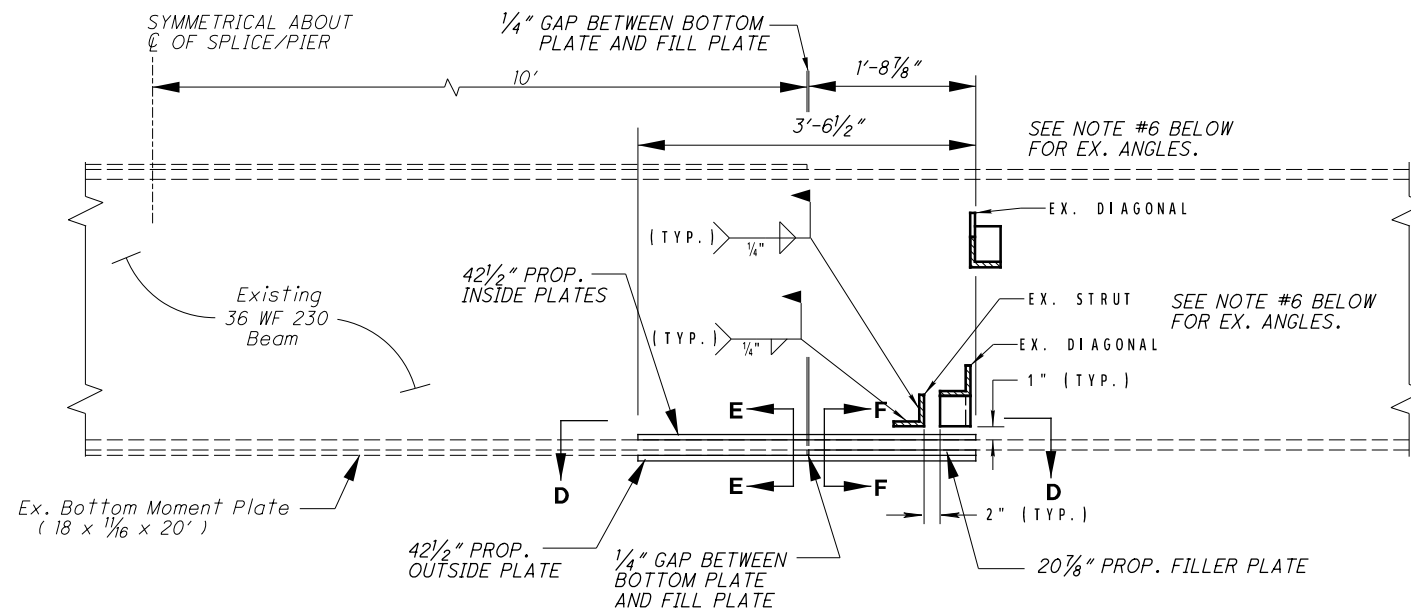
BRIDGE NO. : MIA-055-1183  
S.R. 55 OVER GREAT MIAMI RIVER

D07 BH FY20(B)  
PID No. 105417

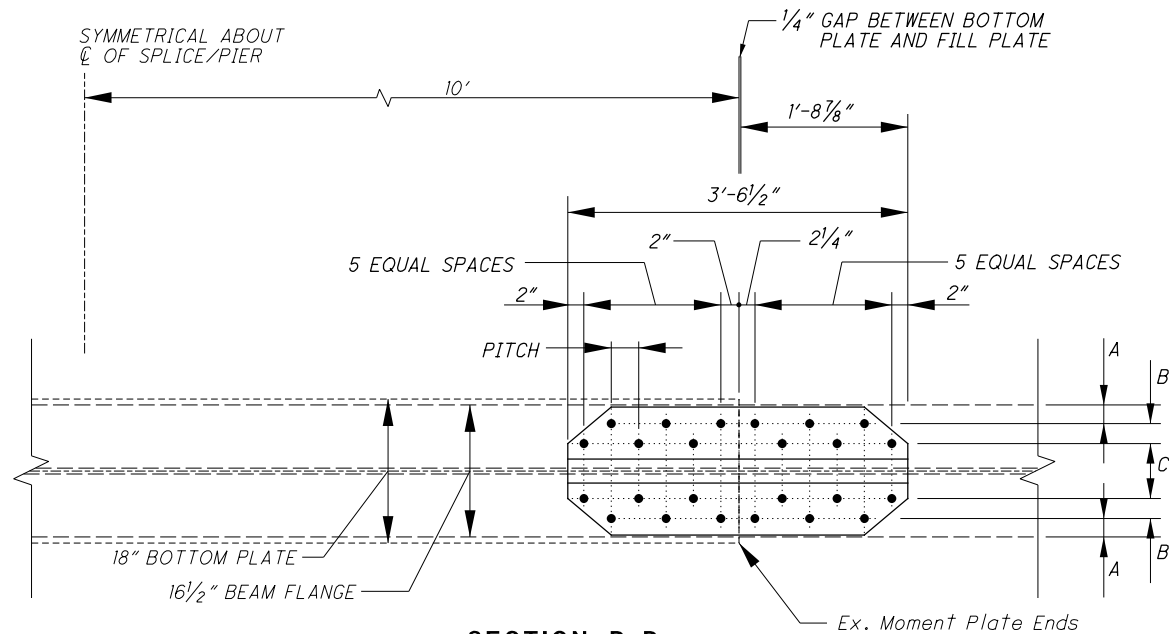
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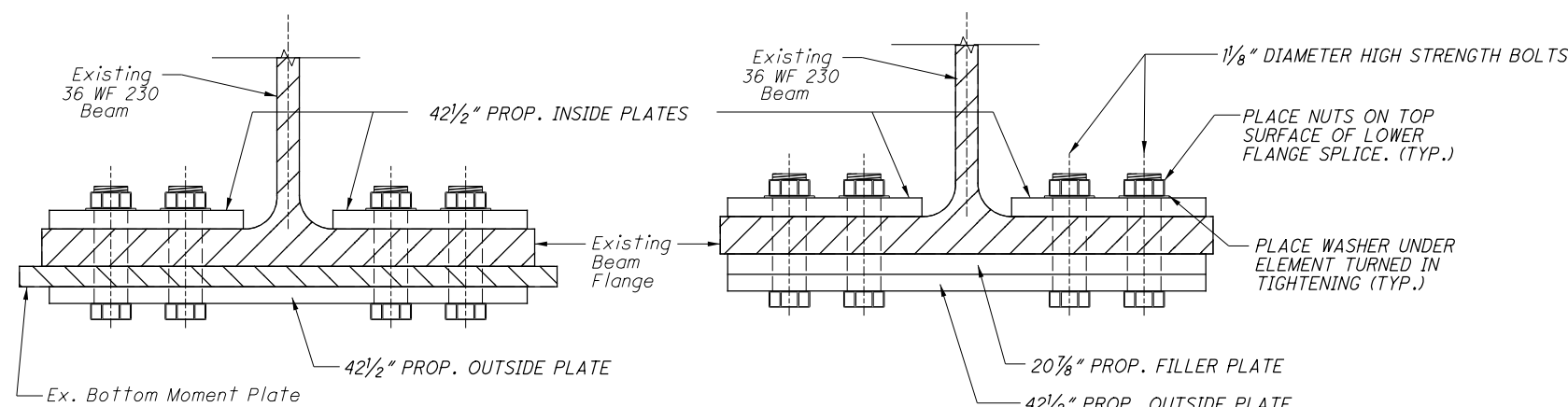
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FLANGE SPLICE - TYPE B



SECTION D-D

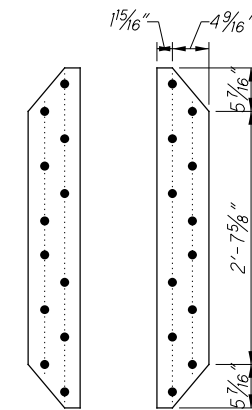


SECTION E-E

SECTION F-F

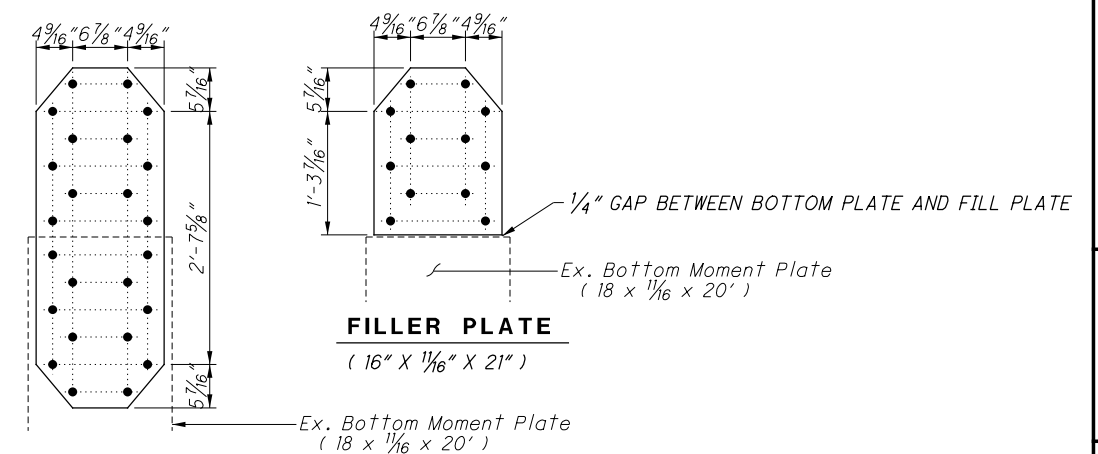
INSIDE PLATES

2 EACH ( 6 1/2" X 5/8" X 42 1/2" )



OUTSIDE PLATE

( 16" X 9/16" X 42 1/2" )



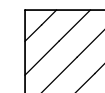
DETAILS ARE NOT TO SCALE

FLANGE SPLICE DETAILS											
BEAM #5	FLANGE PLATES					FLANGE BOLTS					WEIGHT OF SPLICE MAT'L LBS.
	PIER	OUTSIDE	FILLER	INSIDE	NUMBER	# SPACES	PITCH	A	B	C	
		2 REQ'D.	2 REQ'D.	4 REQ'D.							
W36x230	2	16"x9/16"x42 1/2"	16"x1 1/16"x20 7/8"	6 1/2"x5/8"x42 1/2"	24	5	3 3/8"	2 5/16"	2 1/2"	6 7/8"	643.24
W36x230	3	16"x9/16"x42 1/2"	16"x1 1/16"x20 7/8"	6 1/2"x5/8"x42 1/2"	24	5	3 3/8"	2 5/16"	2 1/2"	6 7/8"	643.24
W36x230	4	16"x9/16"x42 1/2"	16"x1 1/16"x20 7/8"	6 1/2"x5/8"x42 1/2"	24	5	3 3/8"	2 5/16"	2 1/2"	6 7/8"	643.24
TOTALS = 1929.72 LBS.											

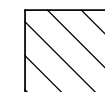
FLANGE SPLICE DETAILS											
BEAM #6	FLANGE PLATES				FLANGE BOLTS						WEIGHT OF SPLICE MAT'RL LBS.
	PIER	OUTSIDE	FILLER	INSIDE	NUMBER	SPACES #	PITCH	A	B	C	
		2 REQ'D.	2 REQ'D.	4 REQ'D.							
W36x230	2	16"x9/16"x42 1/2"	16"x1 1/16"x20 7/8"	6 1/2"x5/8"x42 1/2"	24	5	3 3/8"	2 5/16"	2 1/2"	6 7/8"	643.24
W36x230	3	16"x9/16"x42 1/2"	16"x1 1/16"x20 7/8"	6 1/2"x5/8"x42 1/2"	24	5	3 3/8"	2 5/16"	2 1/2"	6 7/8"	643.24
W36x230	4	16"x9/16"x42 1/2"	16"x1 1/16"x20 7/8"	6 1/2"x5/8"x42 1/2"	24	5	3 3/8"	2 5/16"	2 1/2"	6 7/8"	643.24
TOTALS = 1929.72 LBS.											

NOTES AND SYMBOLS

- 1). ALL FASTENERS ARE 1/8" DIAMETER HIGH STRENGTH BOLTS CONFORMING TO ASTM A-325.
- 2). ALL BOLTS SHALL BE PLACED IN A STAGGERED PATTERN AS SHOWN.
- 3). ALL SHAPES AND PLATES SHALL BE DESIGNATED (CVN), AND SHALL MEET SPECIFIED MINIMUM NOTCH TOUGHNESS REQUIREMENTS AS SPECIFIED IN 711.01 OF CMS.
- 4). APPLICATION SHOWN IS FOR ONE END OF THE MOMENT PLATE, THE OTHER END SHALL BE TREATED THE SAME. A TOTAL OF (12) MOMENT PLATE ENDS SHALL BE REPAIRED, ALONG BEAM #5 and #6 OVER PIERS 2,3, AND 4.
- 5). TABULATED WEIGHTS ARE APPROXIMATE AND ARE FOR ESTIMATING PURPOSES ONLY. THEY INCLUDE AND ALLOWANCE FOR WEIGHT OF BOLTS AND WASHERS.
- 6). ANY CROSSFRAME ANGLE FOUND TO CONFLICT THE INSTALLATION OF THE PLATES AND BOLTS SHALL BE REMOVED AND REINSTALLED. GRIND WELDS SMOOTH AND REPAIR DAMAGED PAINT. THERE ARE 4 EXPECTED CONFLICT LOCATIONS. PAYMENT FOR THE RELOCATION OF EXISTING CROSS FRAME ANGLES TO BE INCLUDED WITH ITEM 513-STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN

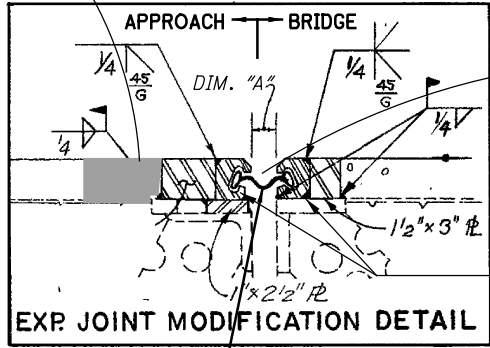
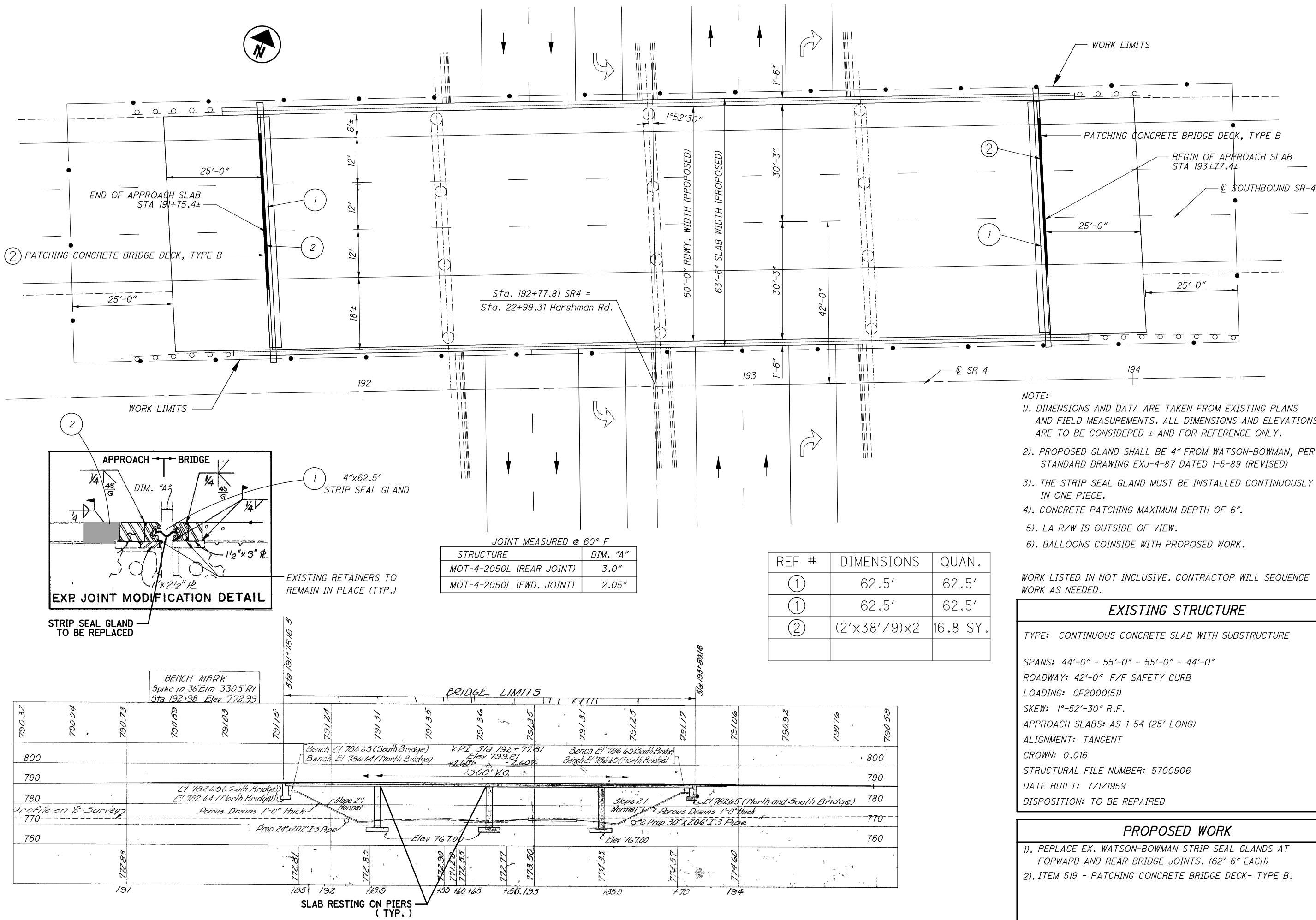


= EXISTING LOWER PORTION OF BEAM AND FLANGE



= EXISTING LOWER MOMENT PLATE

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1 4"x62.5'  
STRIP SEAL GLAND

EXISTING RETAINERS TO  
REMAIN IN PLACE (TYP.)

JOINT MEASURED @ 60° F	
STRUCTURE	DIM. "A"
MOT-4-2050L (REAR JOINT)	3.0"
MOT-4-2050L (FWD. JOINT)	2.05"

REF #	DIMENSIONS	QUAN.
①	62.5'	62.5'
①	62.5'	62.5'
②	(2'x38'/9)x2	16.8 SY.

- NOTE:
- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
  - 2). PROPOSED GLAND SHALL BE 4" FROM WATSON-BOWMAN, PER STANDARD DRAWING EXJ-4-87 DATED 1-5-89 (REVISED)
  - 3). THE STRIP SEAL GLAND MUST BE INSTALLED CONTINUOUSLY AND IN ONE PIECE.
  - 4). CONCRETE PATCHING MAXIMUM DEPTH OF 6".
  - 5). LA R/W IS OUTSIDE OF VIEW.
  - 6). BALLOONS COINSIDE WITH PROPOSED WORK.

WORK LISTED IN NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

### EXISTING STRUCTURE

TYPE: CONTINUOUS CONCRETE SLAB WITH SUBSTRUCTURE

SPANS: 44'-0" - 55'-0" - 55'-0" - 44'-0"

ROADWAY: 42'-0" F/F SAFETY CURB

LOADING: CF2000(51)

SKEW: 1°-52'-30" R.F.

APPROACH SLABS: AS-1-54 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 0.016

STRUCTURAL FILE NUMBER: 5700906

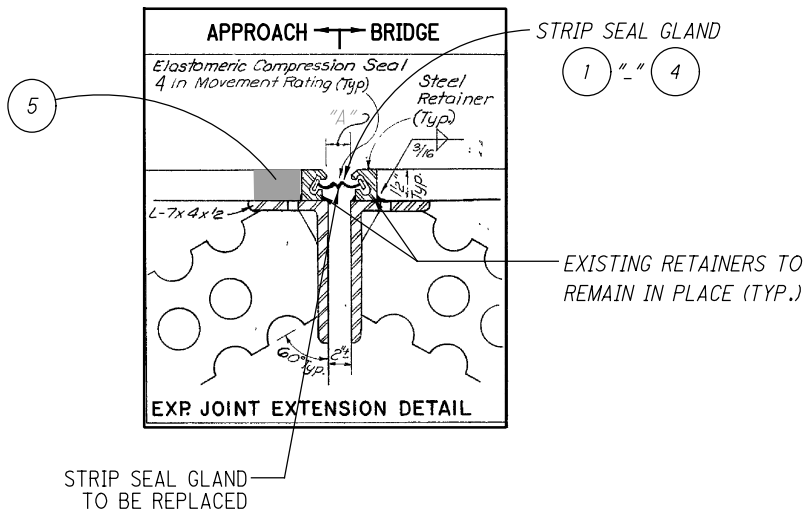
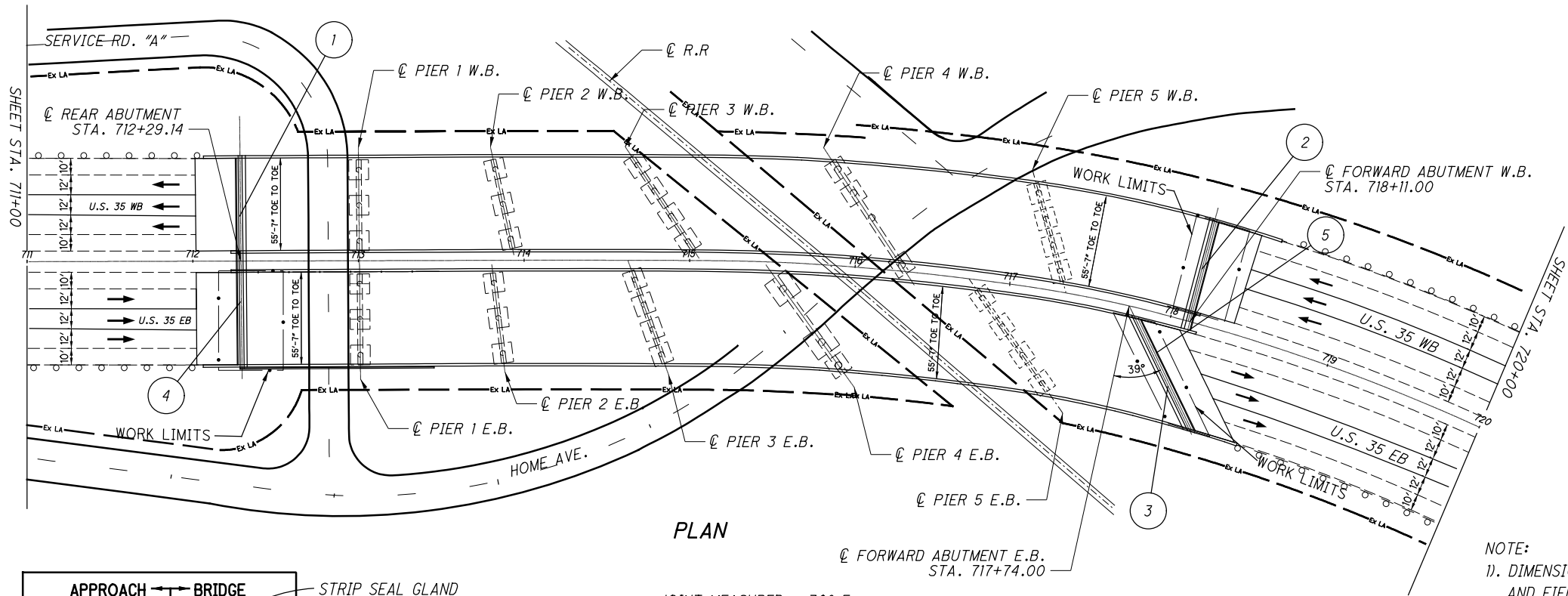
DATE BUILT: 7/1/1959

DISPOSITION: TO BE REPAIRED

### PROPOSED WORK

- 1). REPLACE EX. WATSON-BOWMAN STRIP SEAL GLANDS AT FORWARD AND REAR BRIDGE JOINTS. (62'-6" EACH)
- 2). ITEM 519 - PATCHING CONCRETE BRIDGE DECK- TYPE B.

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JOINT MEASURED @ 30° F	
STRUCTURE	DIM. "A"
MOT-35-1395L (FWD. JOINT)	2.0"
MOT-35-1395L (REAR JOINT)	2.0"
MOT-35-1395R (FWD. JOINT)	2.25"
MOT-35-1395R (REAR JOINT)	2.0"

REF. IN PLAN VIEW

REF #	DIMENSIONS	QUAN.
①	57'-7"	57.58'
②	57'-7"	57.58'
③	77'-0"	77'
④	57'-7"	57.58'
⑤	2'x38'/9	8.4 SY.

- NOTE:
- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
  - 2). PROPOSED GLAND SHALL BE 4" FROM WATSON-BOWMAN, PER STANDARD DRAWING EXJ-4-87 DATED 1-5-89 (REVISED)
  - 3). CONCRETE PATCHING MAXIMUM DEPTH OF 6"
  - 4). BALLOONS ON JONT DETIAL COINCIDE WITH PROPOSED WORK

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

EXISTING STRUCTURE

TYPE: CONTINUOUS COMPOSITE STEEL PLATE GIRDER (A572) WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: WEST BOUND: 72.00'-94.58'-112.56'-123.76'-98.40'-76.78'  
EAST BOUND: 72.00'-80.08'-83.00'-82.56'-123.31'-100.13'

ROADWAY: 2 AT 56'-0" T/T OF PARAPETS

LOADING: HS 20-44 (CASE II) AND ALTERNATE MILITARY LOADING

SKIEW: VARIES, SEE PLAN

APPROACH SLABS: AS-1-81, 25' LONG

ALIGNMENT: SPIRAL TO TANGENT TO SPIRAL TO CROWN: WHERE APPLICABLE, 0.0156' /FT.

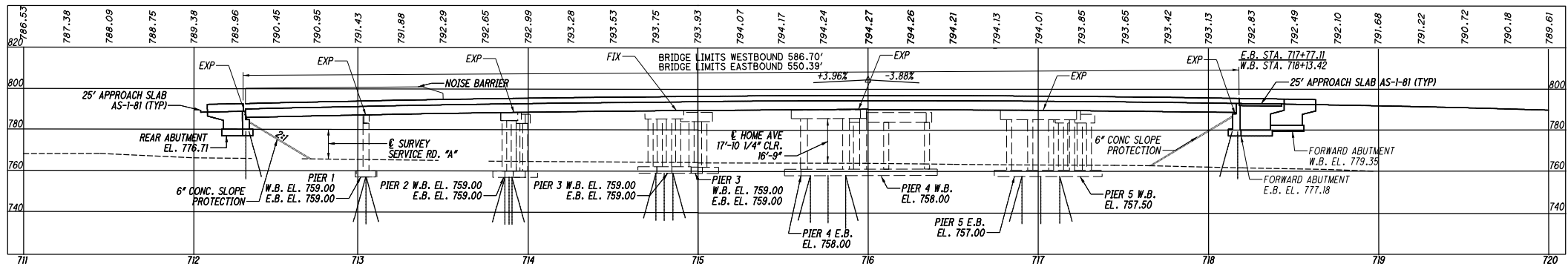
STRUCTURAL FILE NUMBER: 5701600 & 5701619

DATE BUILT: 7/1/1996

DISPOSITION: TO BE REPAIRED

PROPOSED WORK

- 1). REPLACE EX. D.S. BROWNS SEAL GLAND AT FORWARD/REAR EXPANSION JOINTS LEFT. (REAR 57'-7"), (FWD. 57'-7")
- 2). REPLACE EX. D.S. BROWNS SEAL GLAND AT FORWARD/REAR EXPANSION JOINTS RIGHT. (REAR 57'-7"), (FWD. 77' )
- 3). ITEM 519 - PATCHING CONCRETE BRIDGE DECK- TYPE B



SITE PLAN  
BRIDGE NO. : MOT-035-1395 L&R  
OVER FRONTAGE ST., CSX R.R. AND HOME AVENUE

D07 BH FY20(B)  
PID No. 105417

1 / 1

54  
67

MONTGOMERY COUNTY  
STA. 712+29.14  
STA. 718+11.00

DESIGNED  
REB  
CHECKED  
DHG

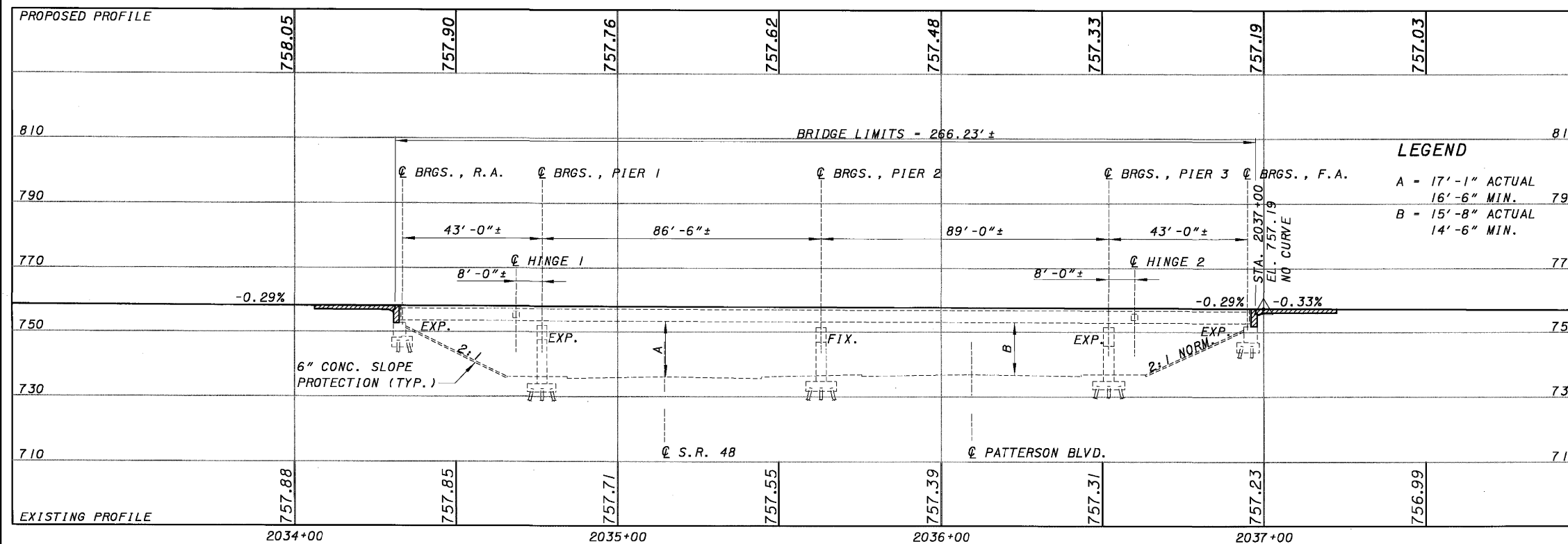
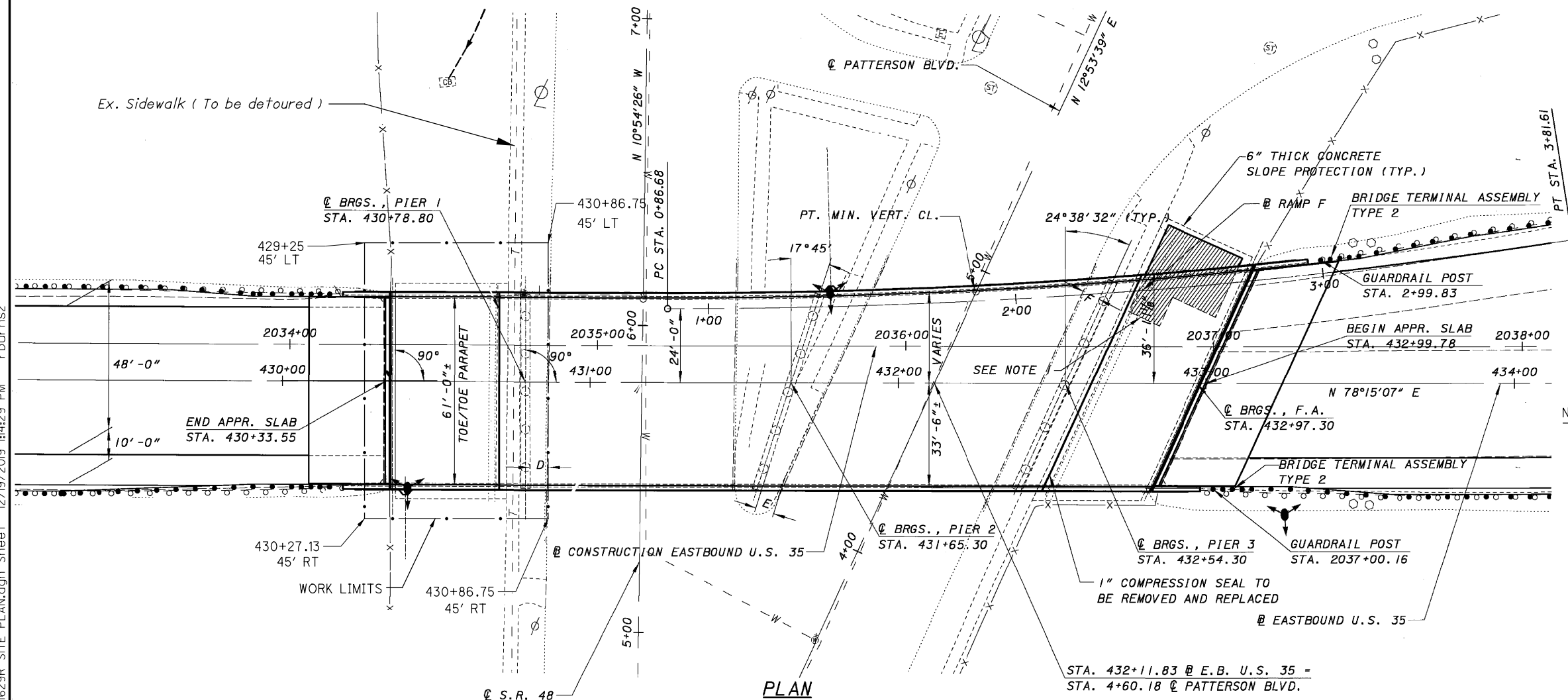
DRAWN  
PUB

REVIEWED  
MRB

DATE  
10/1/19

STRUCTURE FILE NUMBER  
5701600/5701619

DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING



CURVE DATA RAMP F

*P.I. STA. 2+34.44*  
*D = 8°50'52" LT.*  
*D<sub>C</sub> = 3°00'00"*  
*R = 1,909.86'*  
*T = 147.76'*  
*L = 294.93'*  
*E = 5.71'*



NOTES:

- 1). LOCATION FALLS WITHIN L/A RIGHT OF WAY FOR US 35, OUT OF VIEW.
- 2). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED  $\pm$  AND FOR REFERENCE ONLY.
- 3). BEARINGS ARE NUMBERED 1-9 LEFT TO RIGHT LOOKING UPSTATION US-35.
- 4). REAR ABUTMENT BEARINGS ARE BR-75 AND HAVE ANCHORED BASE PLATES. ANY NECESSARY SHIMS WILL NEED TO BE INSTALLED BETWEEN THE BOTTOM FLANGE AND TOP BEARING PLATE.
- 5). SEE SHEET 5 FOR ADDITIONAL NOTES.
- 6). EX. WALK CLOSED ALONG SR-48, SEE PEDESTRIAN DETOUR SHEET 22.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL  
SEQUENCE WORK AS NEEDED.

*EXISTING STRUCTURE*

TYPE: CANTILEVERED TWO SPAN CONTINUOUS WELDED  
STEEL PLATE GIRDER AND SIMPLE SPAN WELDED  
STEEL PLATE GIRDER END SPANS WITH REINFORCED  
CONCRETE DECK AND SUPERSTRUCTURE

SPANS: 43'-0"±, 86'-0"±, 89'-0"±, 43'-0" C/C BEARINGS

ROADWAY: VARIES TOE/TOE PARAPET

LOADING: C.F. = 2000(57)

*SKEW: VARIES*

APPROACH SLABS: AS-1-81 (25' LONG)

**ALIGNMENT:** TANGENT

CROWN: 0.016

STRUCTURAL FILE NUMBER: 5702224

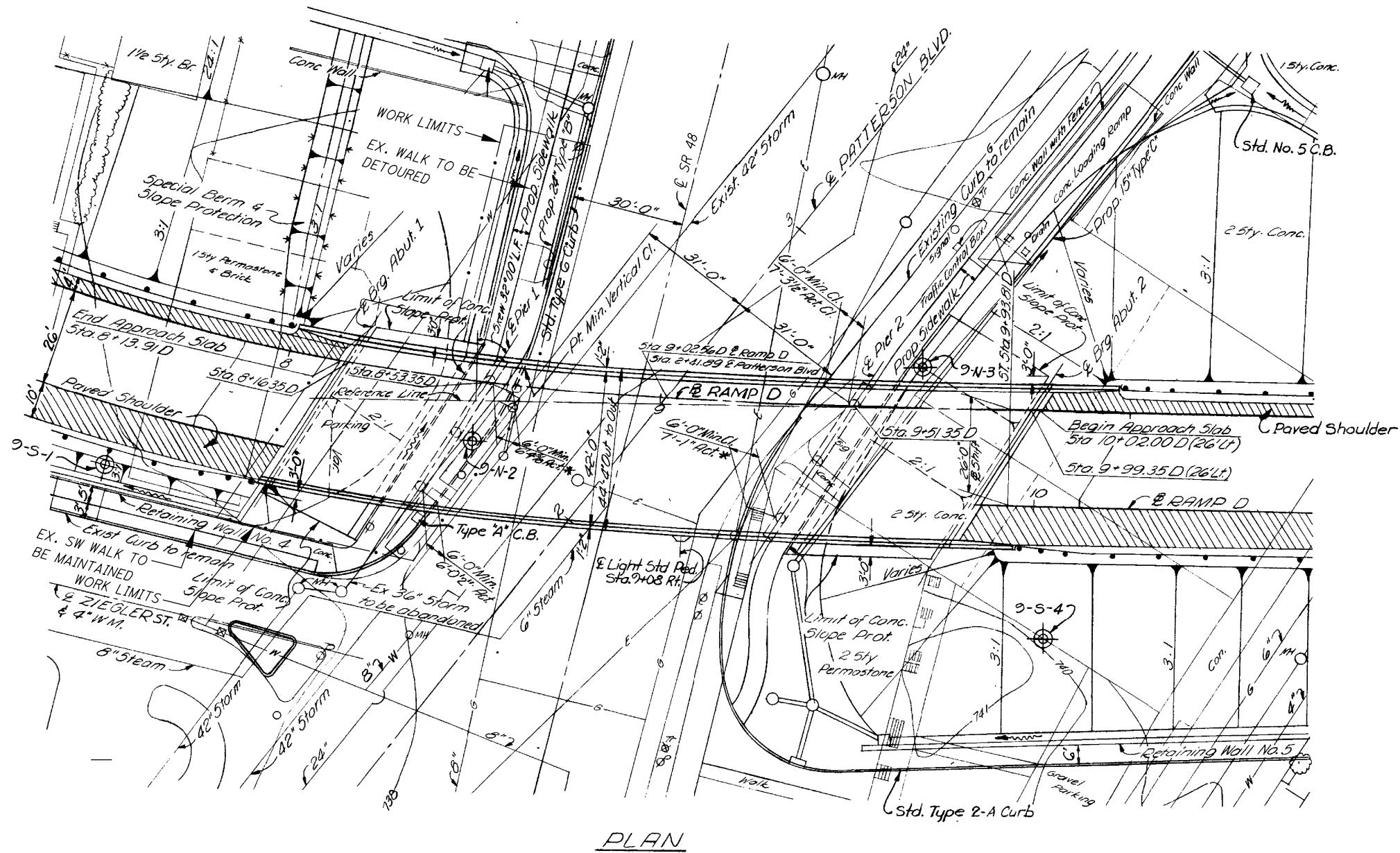
DATE BUILT: 1971

DISPOSITION: TO BE REPAIRED

### PROPOSED WORK

- 1). RESET BEARINGS #6 & #7 AT REAR ABUTMENT

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#### NOTES:

- 1). LOCATION FALLS WITHIN L/A RIGHT OF WAY FOR US 35, OUT OF VIEW.
- 2). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
- 3). BEARINGS ARE NUMBERED 1-6 LEFT TO RIGHT LOOKING UPSTATION ON RAMP "D".
- 4). REAR ABUTMENT BEARINGS ARE R-75.
- 5). STANDARD DRAWING RB-1-55 DATED 2-2-59 (REVISED).
- 6). SEE SHEET 5 FOR ADDITIONAL NOTES.
- 7). FOR PEDESTRIAN DETOUR SEE SHEET 22.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED

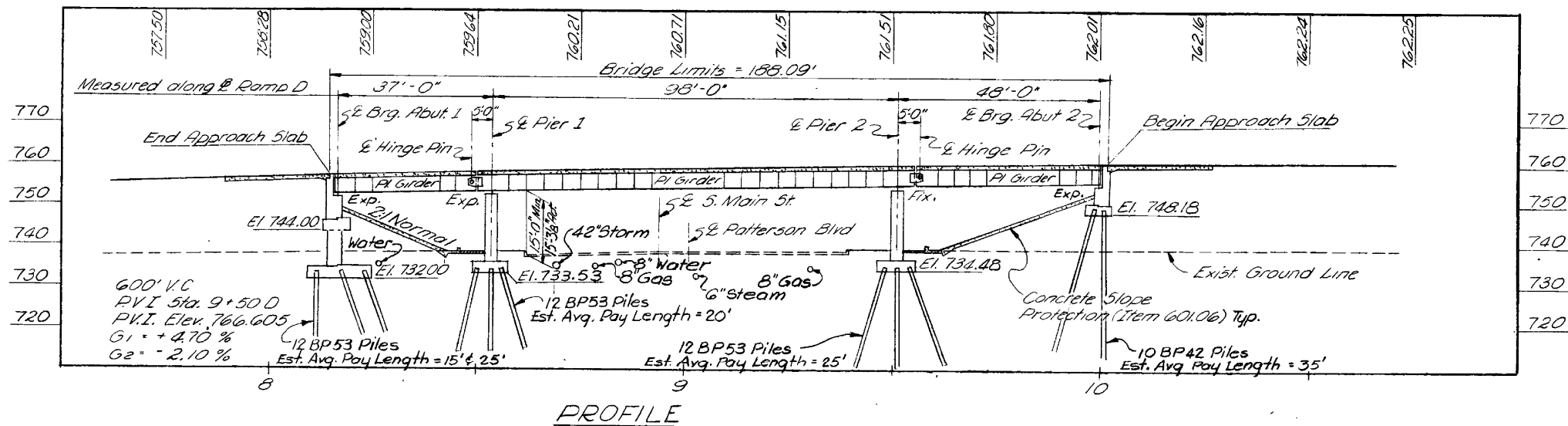
#### EXISTING STRUCTURE

TYPE: CANTILEVERED WELDED STEEL PLATE GIRDER AND SIMPLE SPAN WELDED STEEL PLATE GIRDER END SPANS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: 37'-0", 98'-0", 48'-0" C/C BEARINGS  
ROADWAY: 42'-0" F/F OF PARAPETS  
LOADING: CF-2000(57)  
SKEW: 32° 00' LEFT FORWARD  
APPROACH SLABS: AS-1-54 (25' LONG)  
ALIGNMENT: SPIRAL TO THE LEFT AND TANGENT  
STRUCTURAL FILE NUMBER: 5703646  
DATE BUILT: 7/1/1970  
DISPOSITION: TO BE REPAIRED

#### PROPOSED WORK

- 1). RESET BEARING #5 REAR ABUTMENT



DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
10/3/19  
REVIEWED  
MRB  
STRUCTURE FILE NUMBER  
5703646

DRAWN  
REB  
CHECKED  
DGH  
DESIGNED  
REB

MONTGOMERY COUNTY  
STA. 8+13.91 D  
STA. 10+02.00 D

#### SITE PLAN

BRIDGE NO. : MOT-048-1214  
U.S. 35 RAMP "D" OVER S.R. 48

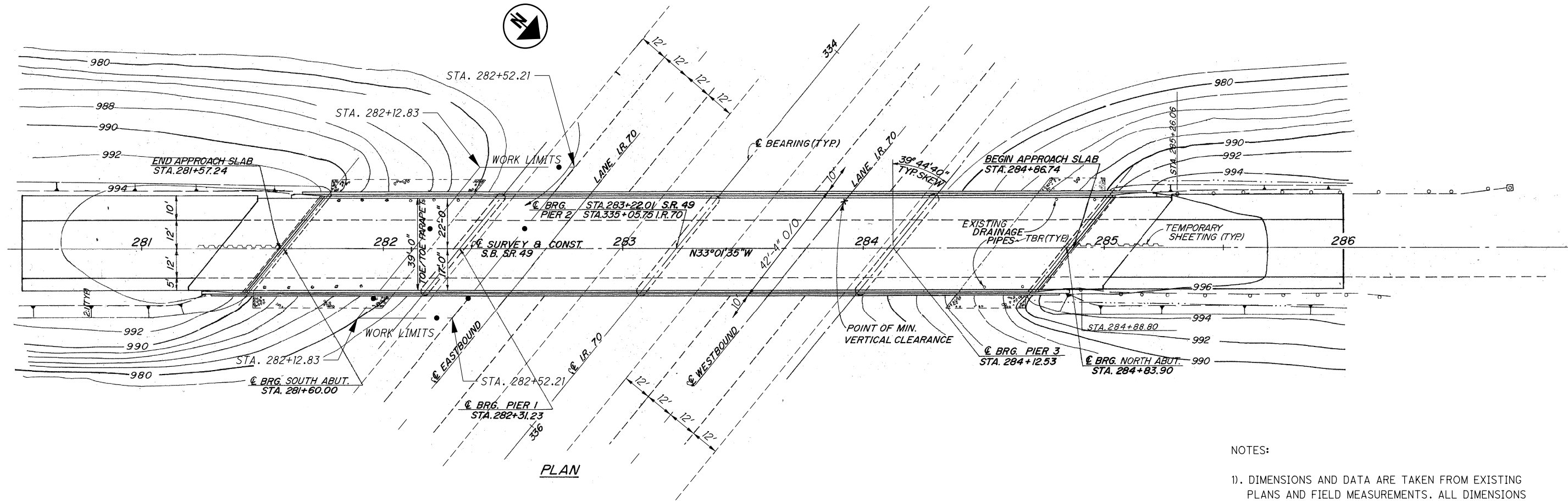
D07 BH FY20(B)  
PID No. 105417

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NOTES:

- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
- 2). BEARINGS ARE NUMBERED 1-6 LEFT TO RIGHT LOOKING UPSTATION SB SR-49.
- 3). PIER 1 BEARINGS ARE R-225.
- 4). STANDARD DRAWING RB-1-55 DATED 3-1-55.
- 5). SEE SHEET 5 FOR ADDITIONAL NOTES.
- 6). R/W LIMITS ARE BEYOND THE VIEW OF THIS LOCATION.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

EXISTING WORK

TYPE: CANTILEVERED TWO SPAN CONTINUOUS WELDED STEEL PLATE GIRDER AND SIMPLE SPAN WELDED STEEL PLATE GIRDER END SPANS WITH REIN. CONCRETE DECK AND SUBST.

SPANS: 43'-0"±, 86'-6"±, 89'-0"±, 43'-0"± C/C BEARINGS

ROADWAY: VARIES TOE/TOE PARAPET

LOADING: C.F. = 2000(57)

SKEW: VARIES

APPROACH SLABS: AS-1-81 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 0.016

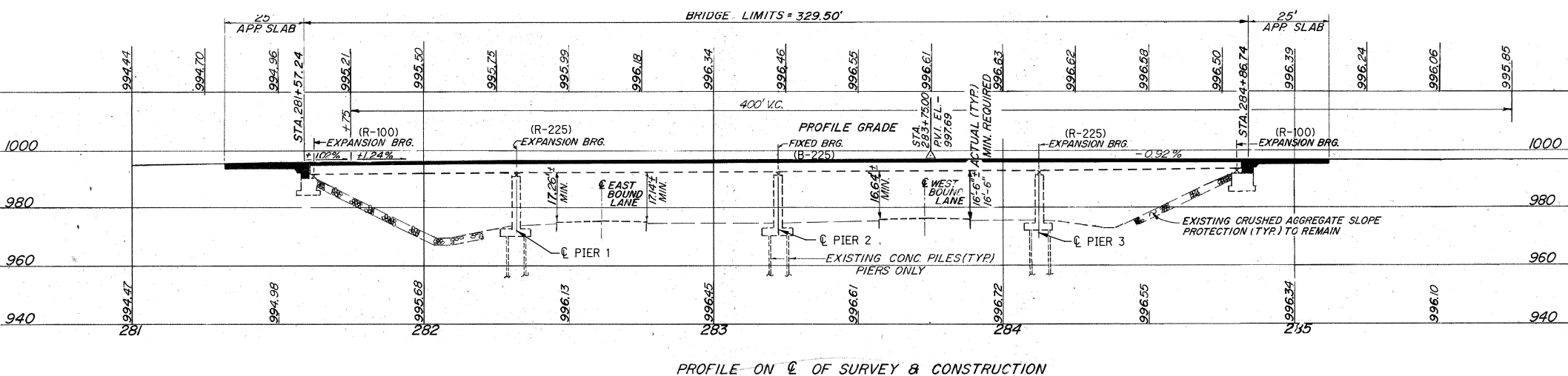
STRUCTURAL FILE NUMBER: 5704502

DATE BUILT: 7/1/1960

DISPOSITION: TO BE REPAIRED

PROPOSED WORK

- 1). RESET BEARINGS #6 PIER 1 AT EASTBOUND I.R.-70.



DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
10/3/19  
REVIEWED  
MRB  
STRUCTURE FILE NUMBER  
5704502

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REVIS  
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DESIGNED  
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DHG  
MONTGOMERY COUNTY  
STA. 281+57.24  
STA. 284+88.80

SITE PLAN

BRIDGE NO. : MOT-049-1091 L  
S.R. 49 OVER I.R. 70

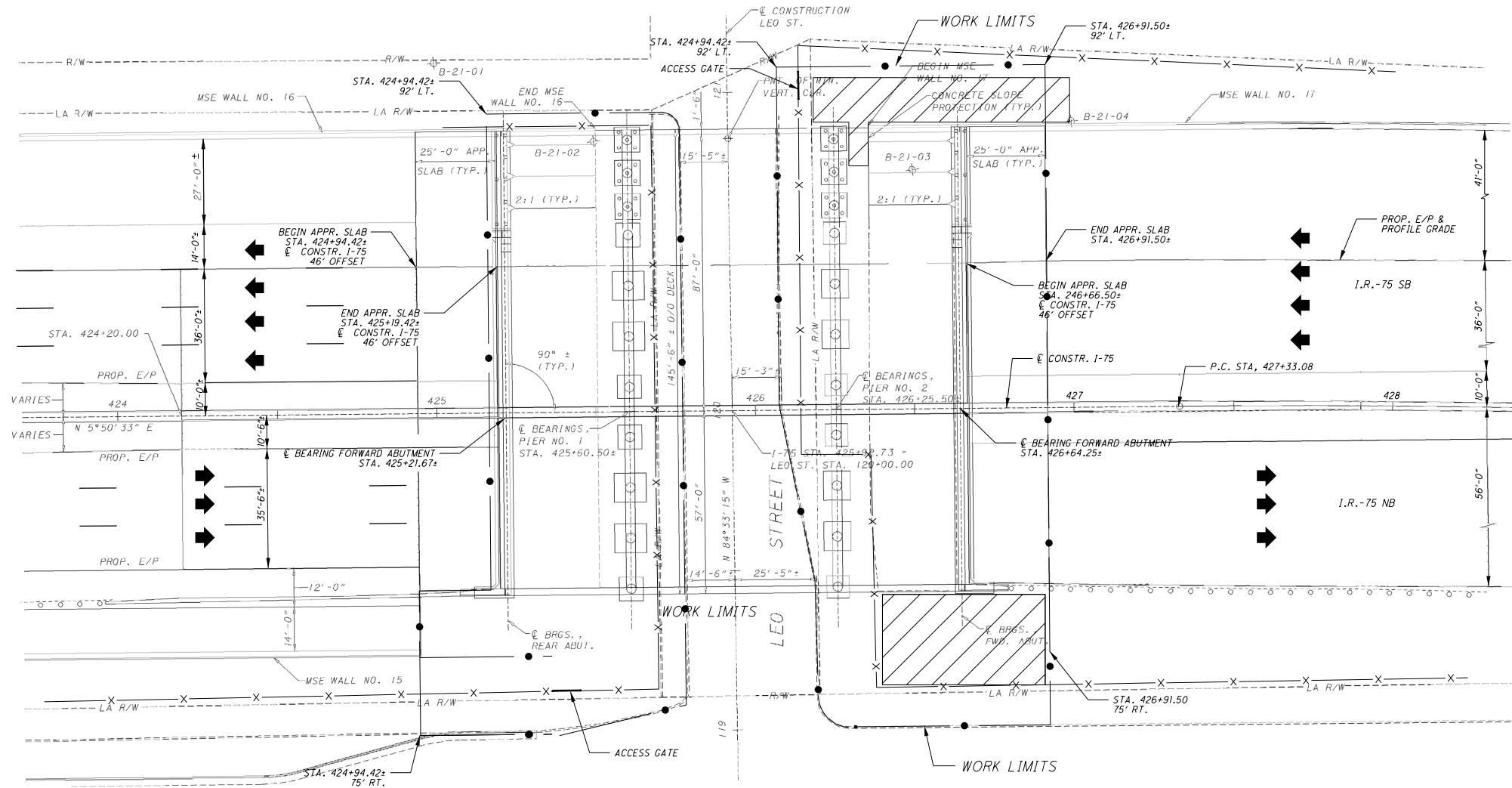
D07 BH FY20(B)  
PID No. 105417

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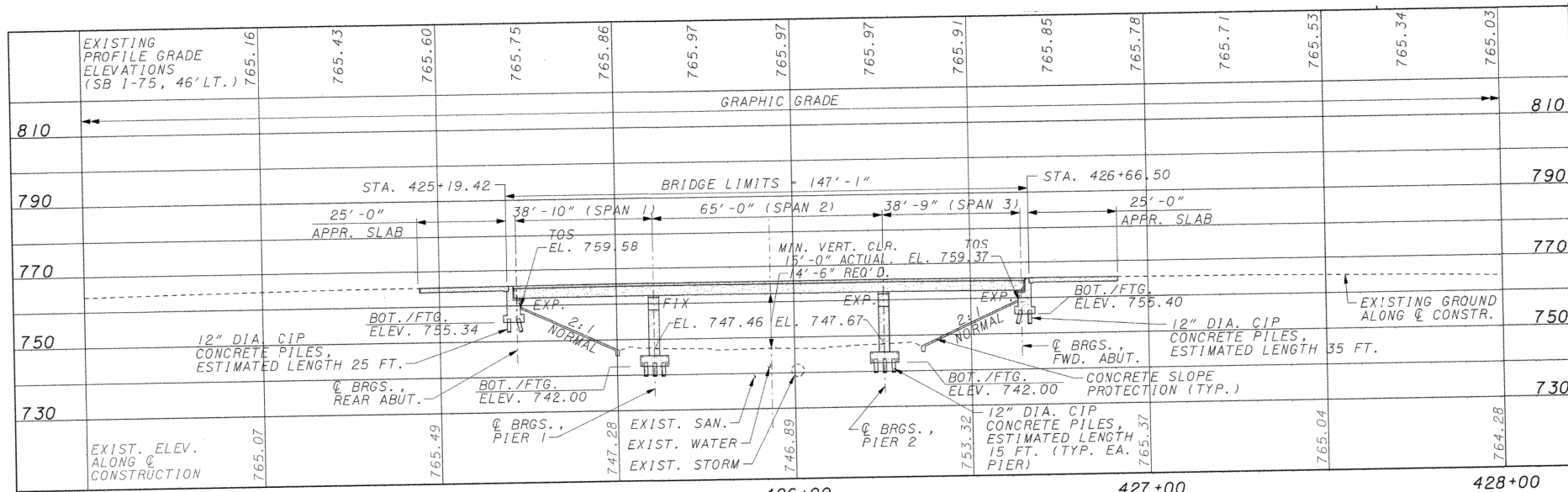
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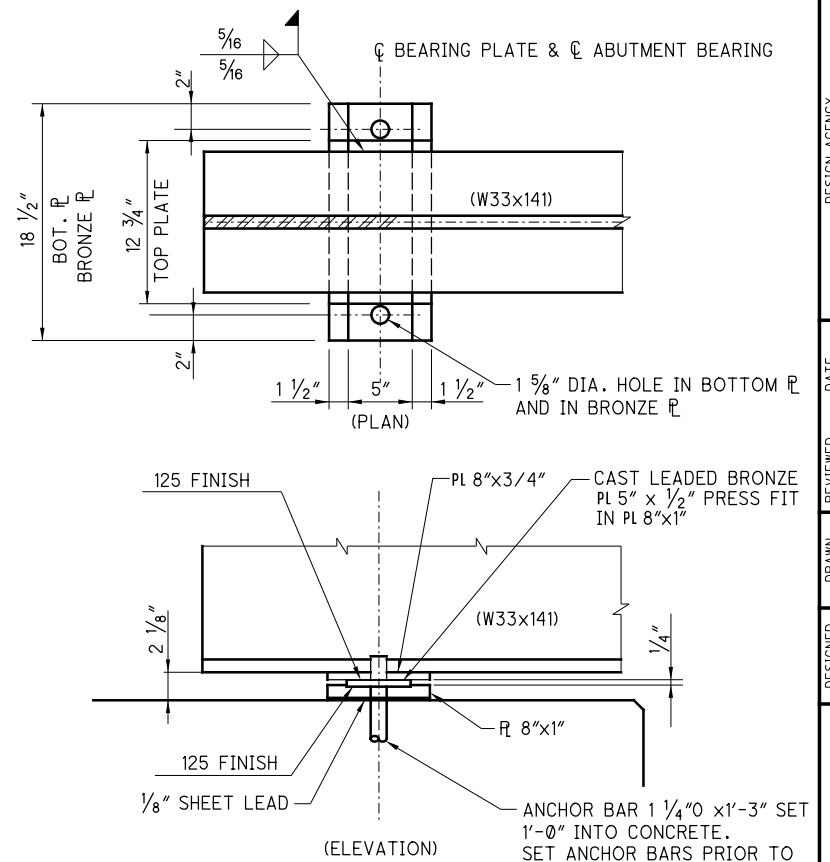
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PLAN




PROFILE ALONG SB PROFILE GRADE LINE, I-75



ABUTMENT BEARING PLATES

NOTES:

- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
- 2). BEARINGS ARE NUMBERED 1-13 LEFT TO RIGHT LOOKING UPSTATION I.R. 75.
- 3). EXISTING ABUTMENT BEARINGS ARE LOW PROFILE EXPANSION BEARINGS.
- 4). SEE SHEET 5 FOR ADDITIONAL NOTES.
- 5). ALL EXISTING WALKS ARE TO BE MAINTAINED.

 = APPROX. AREAS OF CLEARING AND GRUBBING.  
( 20'x 40' NE CORNER ) ( 10'x 60' NW CORNER )

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

### EXISTING STRUCTURE

TYPE: 3 SPAN CONTINUOUS NON-COMPOSITE STEEL BEAMS (ASTM A 709 GR50, PAINTED) WITH REINFORCED CONCRETE DECK, CAP AND COLUMN PIERS, AND STUB ABUTMENTS

SPANS: 38'-10", 65'-0", 38'-9" C/C BEARINGS

ROADWAY: 85'-8" (LB), 53'-8" (RB) TOE TO TOE PARAPETS

LOADING: HS20 (CASE 1) AND THE ALTERNATE MILITARY LOADING

SKEW: NONE

APPROACH SLABS: AS-I-81 (25'-0" LONG)

ALIGNMENT: TANGENT

CROWN: 0.016 FT/FT

STRUCTURAL FILE NUMBER: 5708613

DATE BUILT: 1959

DISPOSITION: TO BE REPAIRED

### PROPOSED WORK

- 1). RESET BEARINGS #1, #6, #7, and #8 REAR ABUTMENT
- 2). RESET BEARINGS #1, #4, #5, and #8 FORWARD ABUTMENT
- 3). CLEARING AND GRUBBING AT FORWARD ABUTMENT.

DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
10/3/19  
REVIEWED  
MRB  
STRUCTURE FILE NUMBER  
5708613

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MONTGOMERY COUNTY  
STA. 424+94.42  
STA. 426+91.50

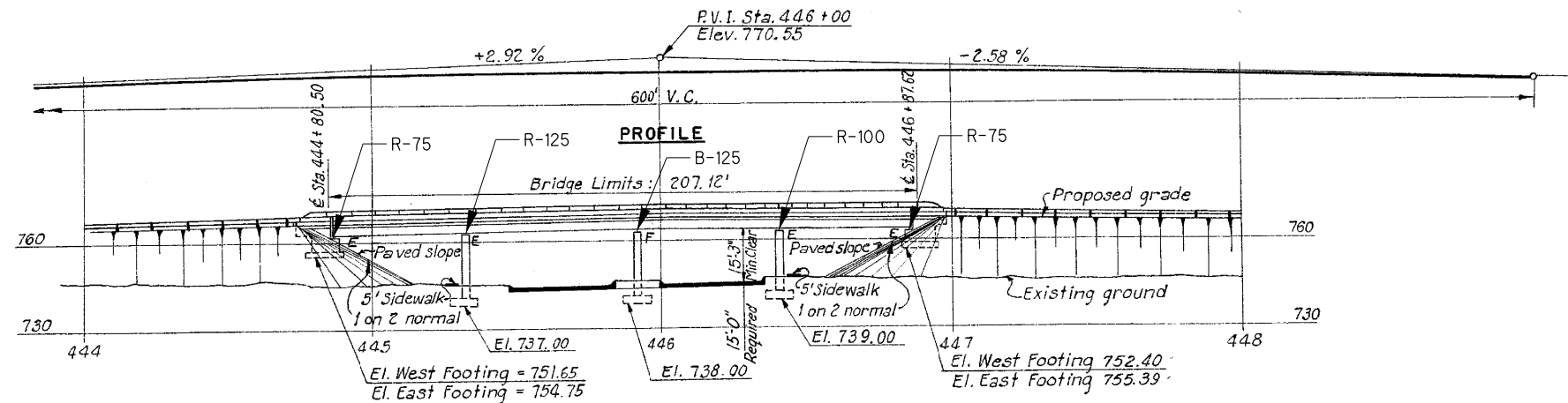
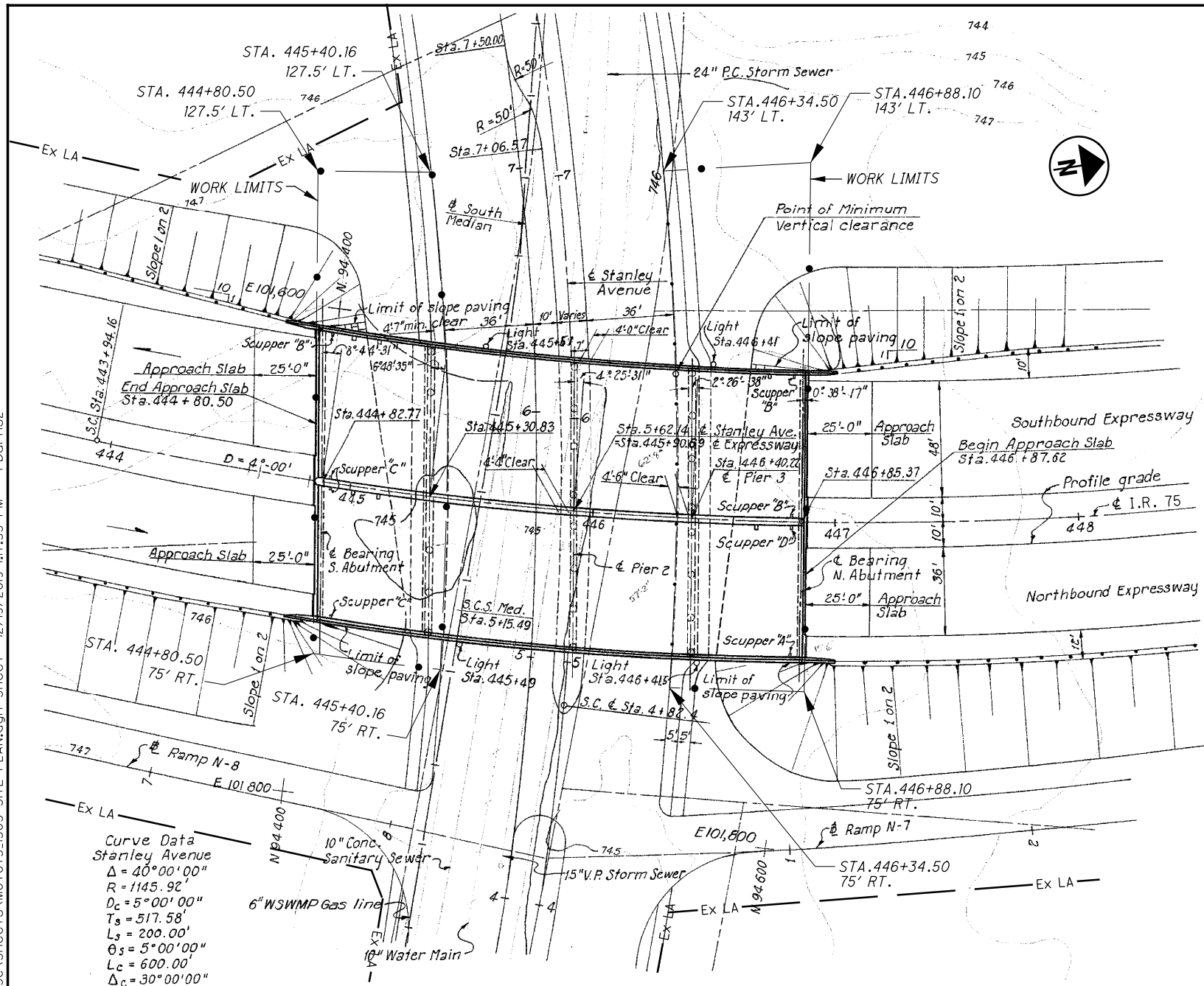
SITE PLAN  
BRIDGE NO. : MOT-075-1466  
I.R. 75 OVER LEO STREET

D07 BH FY20(B)  
PID No. 105417

1 / 1

59  
67

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PROFILE

NOTES:

- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED  $\pm$  AND FOR REFERENCE ONLY.
- 2). BEARINGS ARE NUMBERED 1-20 LEFT TO RIGHT LOOKING UPSTATION IR75.
- 3). EXISTING ABUTMENT BEARINGS ARE R-75 W/SHIMS.
- 4). EXISTING PIER 3 BEARINGS ARE R-100
- 5). SEE SHEET 5 FOR ADDITIONAL NOTES.
- 6). Ex. WALK CLOSED ALONG STANLEY AVE., SEE PEDESTRIAN DETOUR SHEET 28.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

EXISTING STRUCTURE

TYPE: FOUR SPAN CONTINUOUS ROLLED BEAMS  
WITH REINFORCED CONCRETE DECK AND  
SUBSTRUCTURE

SPANS: 48.06', 59.86', 49.53', 45.15'  
ROADWAY: 120'-0" F/F SAFETY CURB  
LOADING: CF2000 (51)  
SKEW:  $4^\circ 25' 31''$  R.F.  
APPROACH SLABS: 4 @ AS-1-54 ; 25' LONG  
ALIGNMENT:  $4^\circ$  CURVE LEFT  
CROWN: 0.0625  
STRUCTURAL FILE NUMBER: 5708648  
DATE BUILT: 7/1/1959  
DISPOSITION: TO BE REPAIRED

PROPOSED WORK

- 1). REPLACE BEARING #1-#20, AT REAR ABUTMENT.
- 2). REPLACE BEARING #1-#20 AT FWD. ABUTMENT.
- 3). RESET BEARINGS #1-#10, #18, #19 ON PIER 3.

DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
10/3/19  
REVIEWED  
MRB  
STRUCTURE FILE NUMBER  
5708648

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REB  
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DHG  
MONTGOMERY COUNTY  
STA. 444+80.50  
STA. 446+87.62

SITE PLAN

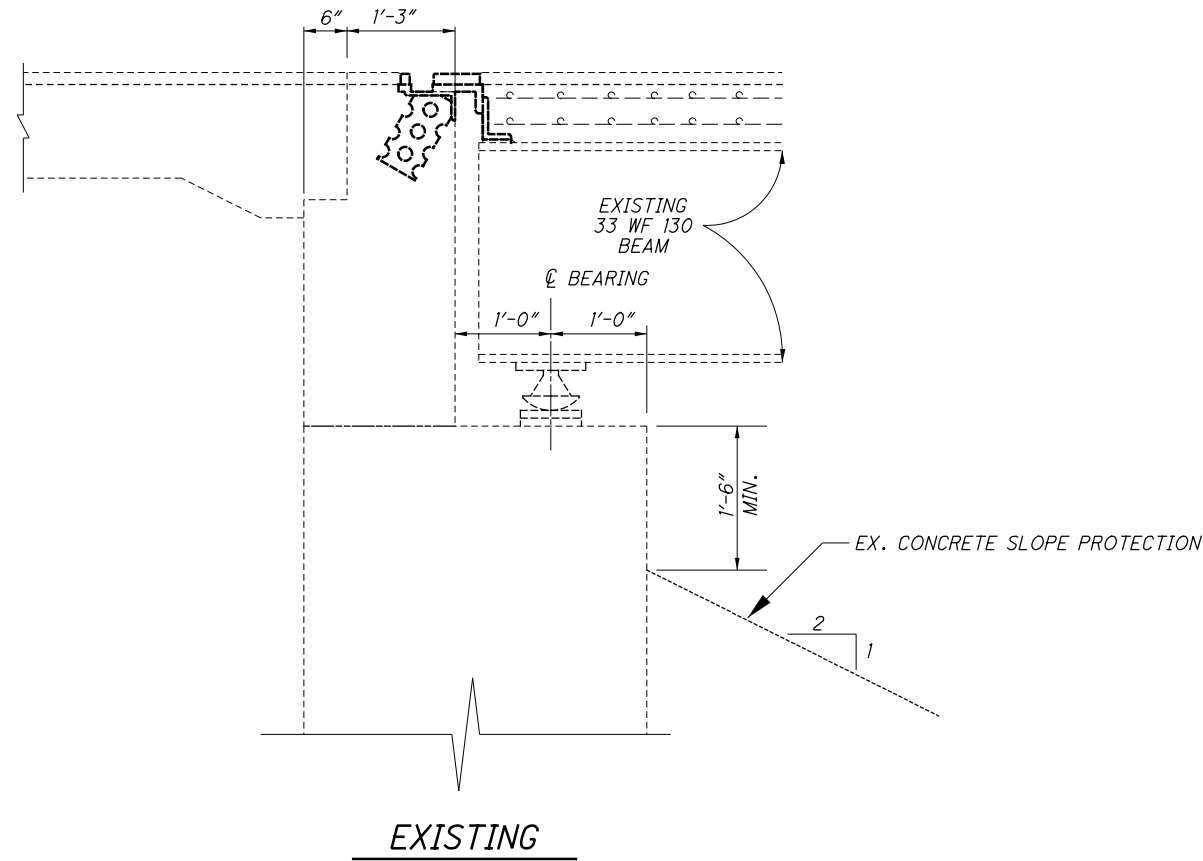
BRIDGE NO. : MOT-075-1503  
I.R. 75 OVER STANLEY AVENUE

D07 BH FY20(B)  
PID No. 105417

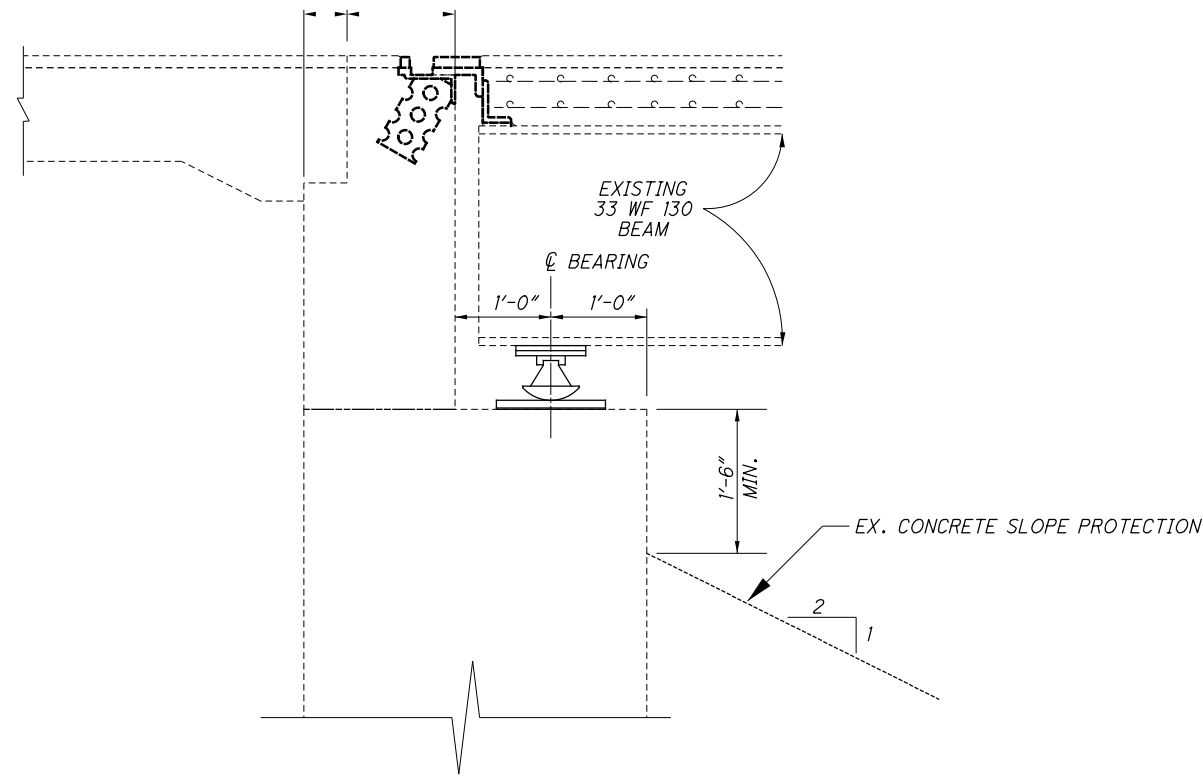
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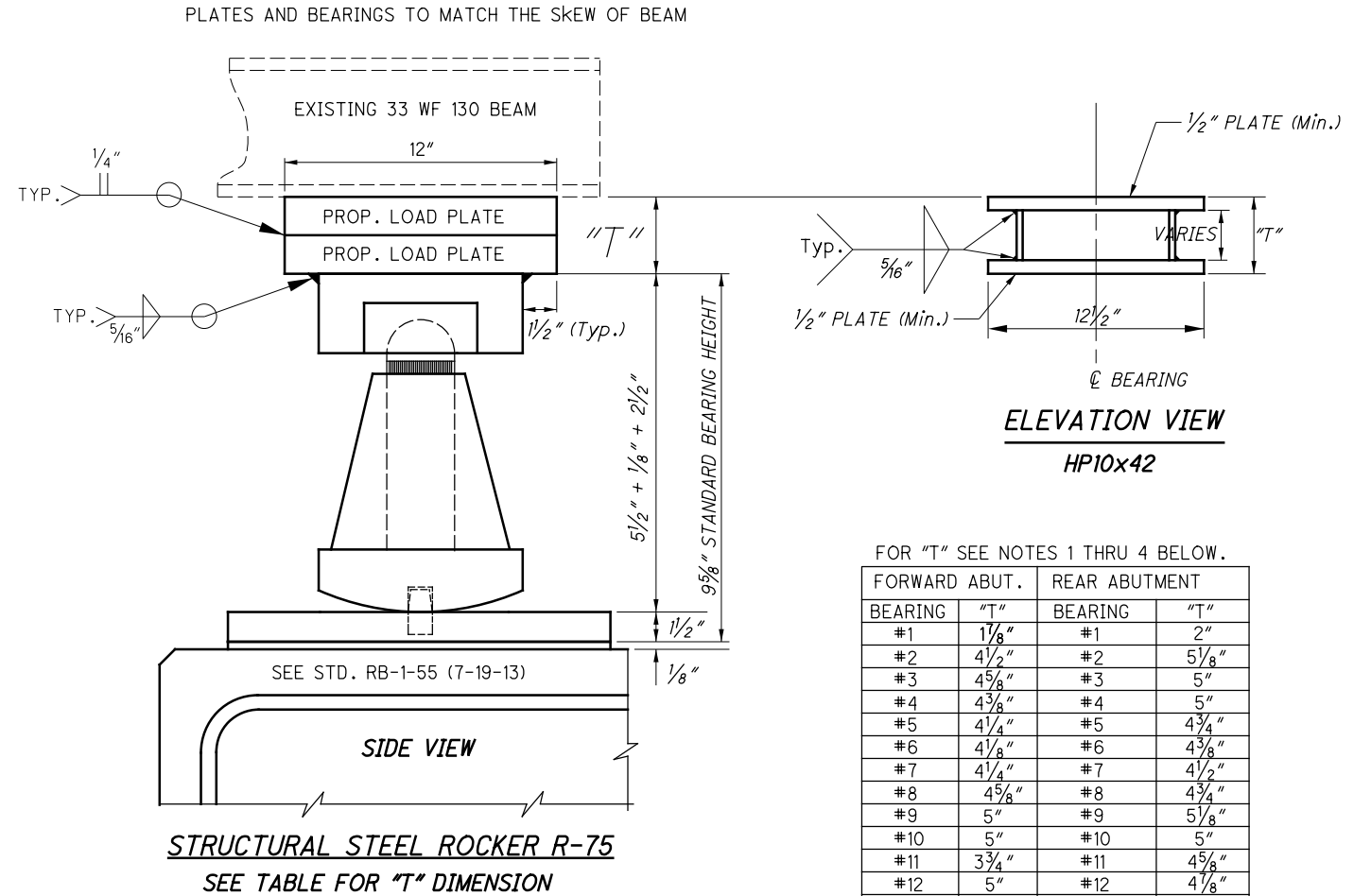
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EXISTING



PROPOSED



- NOTES:
- 1). LOAD PLATES ARE USED AT TOP OF BEARING WHEN "T" EQUALS 4" OR LESS, PLATES WILL VARY IN THICKNESS.
  - 2). H-PILE, STEEL PLATES ARE USED AT TOP OF BEARING WHEN "T" EQUALS GREATER THAN 4", AND WILL VARY IN THICKNESS.
  - 3). "T" = FIELD MEASURED BEARING OPENINGS, (-) MINUS THE PROPOSED STANDARD BEARING HEIGHT AND 1/8" BEARING PAD.
  - 4). BEFORE FABRICATION BEGINS THE CONTRACTOR SHALL BE RESPONSIBLE TO FIELD VERIFY EACH MEASURED BEARING OPENINGS. VERIFICATION TO BE PROVIDED TO ENGINEER FOR ACCEPTANCE.

BEARING DETAILS

BRIDGE NO.: MOT-075-1503  
I.R. 75 OVER STANLEY AVENUE

D07-BH-FY20(B)  
PID No. 105417

2 / 2

61  
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DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

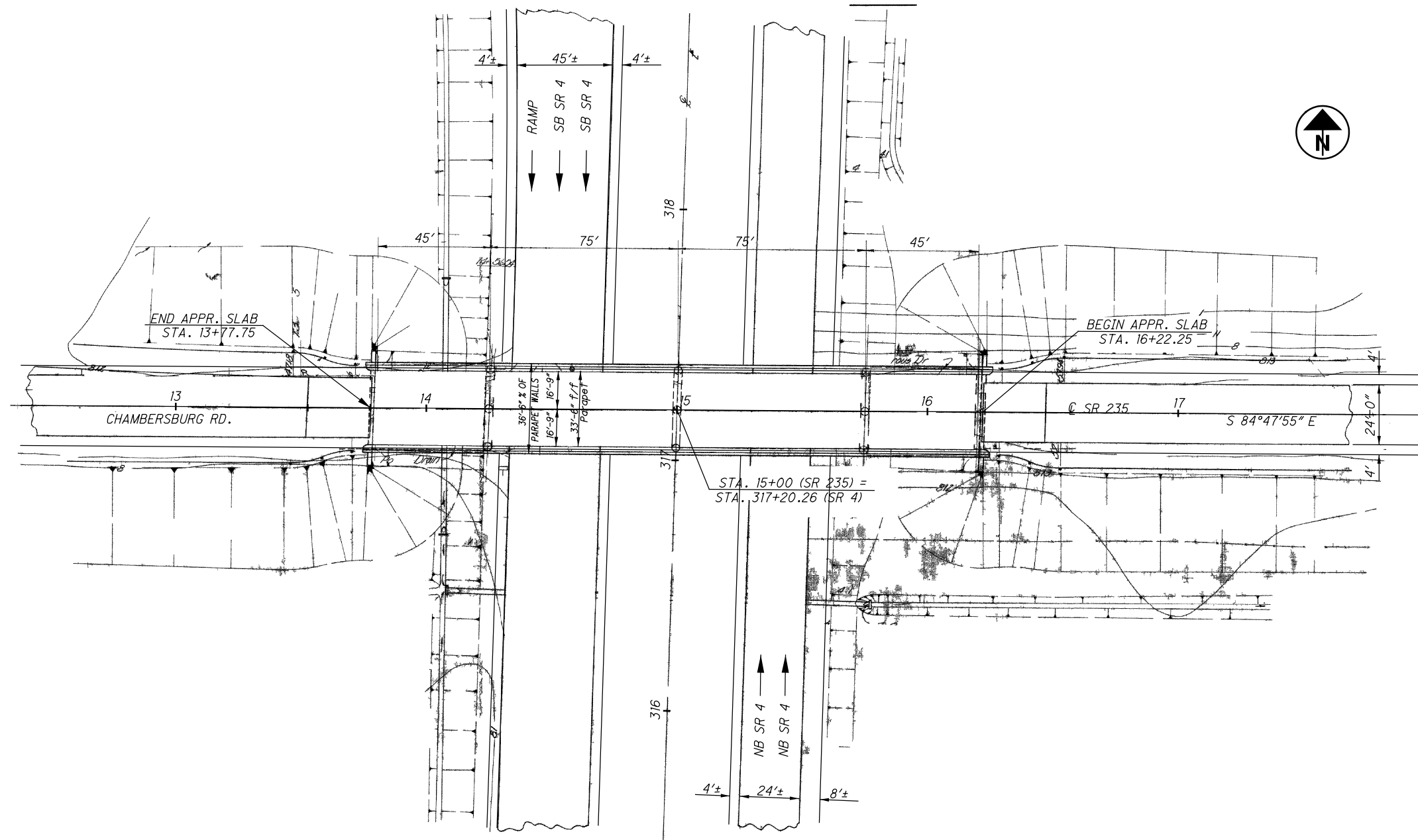
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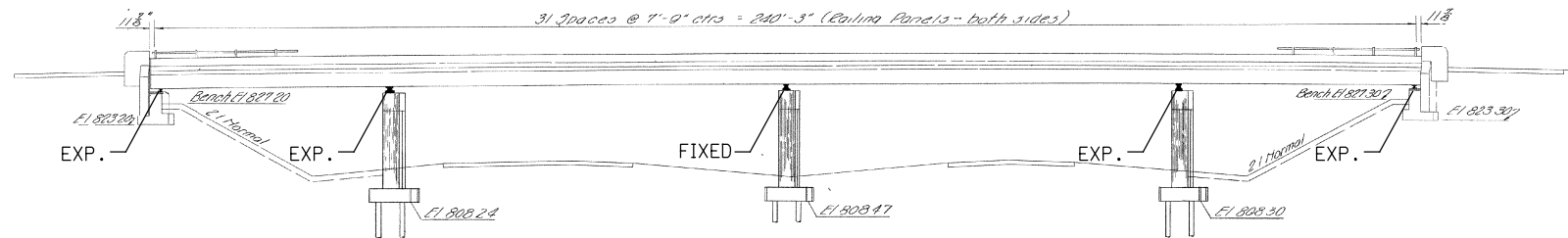
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DATE  
10/3/19

STRUCTURE FILE NUMBER  
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PLAN



PROFILE

NOTES:

- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
- 2). STRUCTURE SHOWN FALLS WITHIN EXISTING RIGHT OF WAY FOR SR. 235/CHAMBERSBURG ROAD.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL BEAM WITH CONCRETE DECK AND SUBSTRUCTURE

SPANS: 45' - 75' - 75' - 45'

ROADWAY: 33'-6" F/F PARAPETS

LOADING: CF 400 (51)

SKEW: 1°-39'-15" L.F.

APPROACH SLABS: AS-1-54 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 0.071 FT./FT.

STRUCTURAL FILE NUMBER: 5709660

DATE BUILT: 1959

DISPOSITION: REPAIR CRASH DAMAGE

PROPOSED WORK

- 1). REMOVE AND REPLACE IMPACT DAMAGED INTERMEDIATE CROSSFRAMES.

SITE PLAN  
BRIDGE NO. : MOT-235-0022L  
S.R. 235 (CHAMBERSBURG RD.) OVER S.R. 4

D07 BH FY20(B)  
PID No. 105417

1 / 2

62  
67

MONTGOMERY COUNTY  
STA. 13+77.75  
STA. 16+22.25

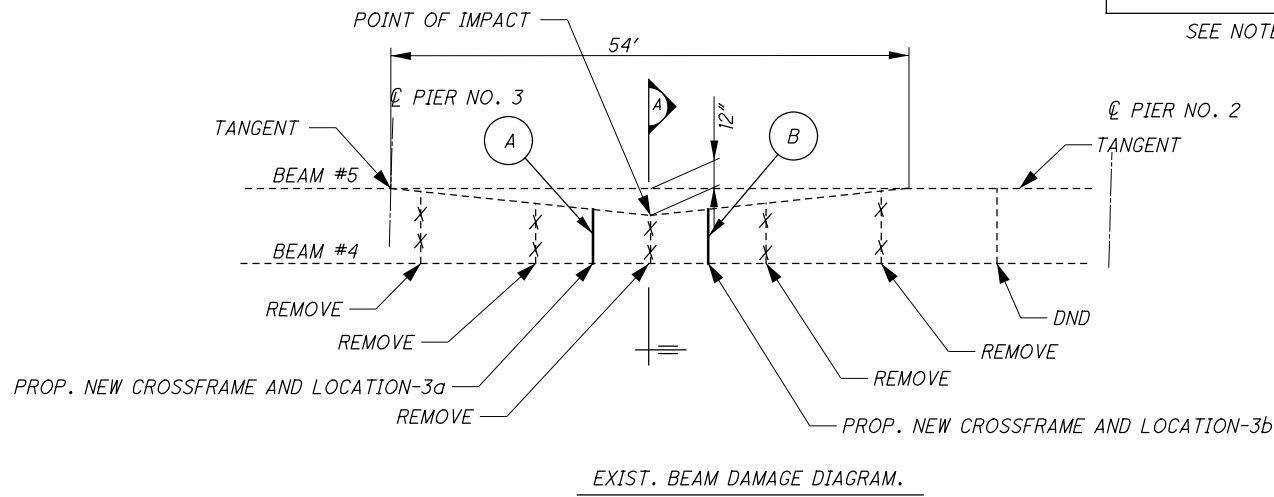
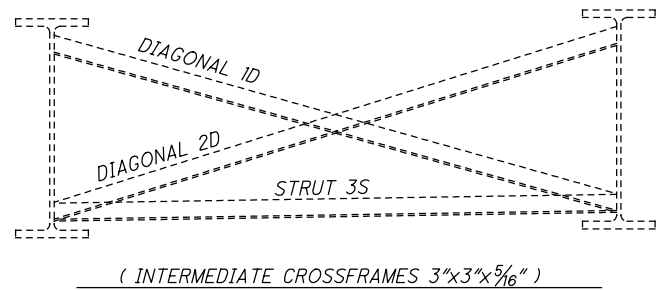
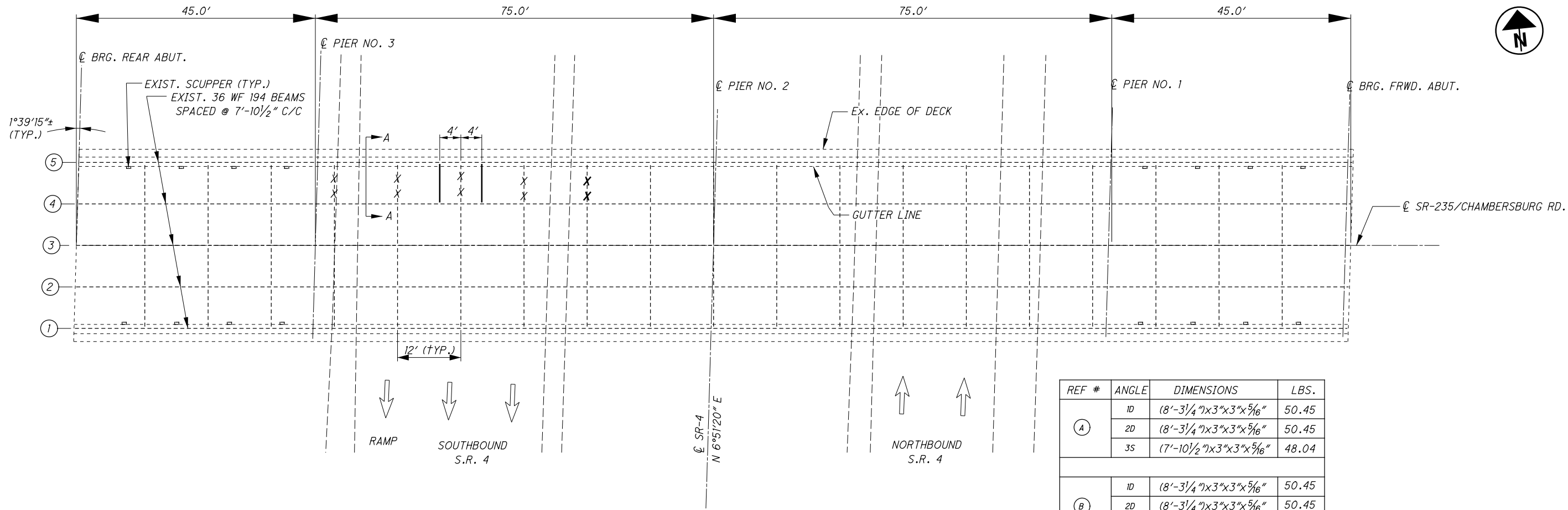
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STRUCTURE FILE NUMBER  
5709660

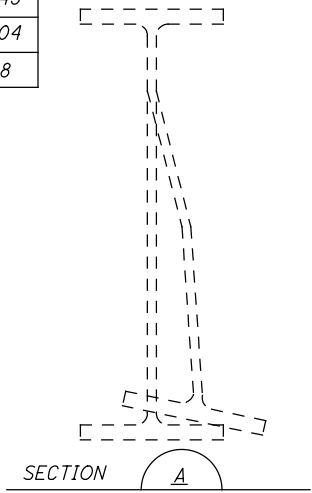
DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

I:\ProjectData\07\105417\_BH\_FY20\_B\Design\Structures\M0T235\_0022L\Sheets\M0T235\_0022L STEEL REPAIR.dgn Sheet 12/19/2019 11:18:42 PM rburns2



REF #	ANGLE	DIMENSIONS	LBS.
A	1D	(8'-3 1/4")x3"x3"x 5/16"	50.45
	2D	(8'-3 1/4")x3"x3"x 5/16"	50.45
	3S	(7'-10 1/2")x3"x3"x 5/16"	48.04
TOTALS			298

SEE NOTE #2 BELOW



SECONDARY MEMBER DIAPHRAGM

MAIN AND SECONDARY MEMBER DAMAGE IS NOT SHOWN FOR CLARITY

SECONDARY MEMBERS TO BE REMOVED - CROSSFRAMES					
BETWEEN BEAMS	BETWEEN PIERS	N	1D	2D	3S
4-5	2-3	1-5	5	5	5
SECONDARY MEMBERS TO BE REPLACED - CROSSFRAMES					
4-5	2-3	3a-3b	2	2	2

N = NUMBER OF CROSSFRAME BRACES COUNTED FROM THE PIER OR ABUTMENT IDENTIFIED IN TABLE.

NOTES AND LEGEND

- 1). REMOVE ACCORDING TO ITEM 202 - PORTIONS OF STRUCTURE REMOVED, OVER 20' SPAN, AS PER PLAN ( SECONDARY MEMBERS )
- 2). REPLACEMENT OF INTERMEDIATE CROSSFRAMES SHALL BE FIELD CUT TO FIT.
- 3). SEE STD. DWG. GSD-1-96 FOR ADDITONAL CLARIFICATION.
- 4). INTERMEDIATE CROSSFRAMES ( L3"x3"x5/16" )
- 5). BEAMS ARE NUMBERED RIGHT TO LEFT 1-5 ( 36WF194 ).

REPAIR DETAILS

BRIDGE NO. MOT-235-0022L  
S.R. 235 (CHAMBERSBURG RD.) OVER S.R. 4

D07-BH-FY20(B)

PID No. 105417

DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
10/9/19  
REVIEWED  
MRB  
STRUCTURE FILE NUMBER  
5710604

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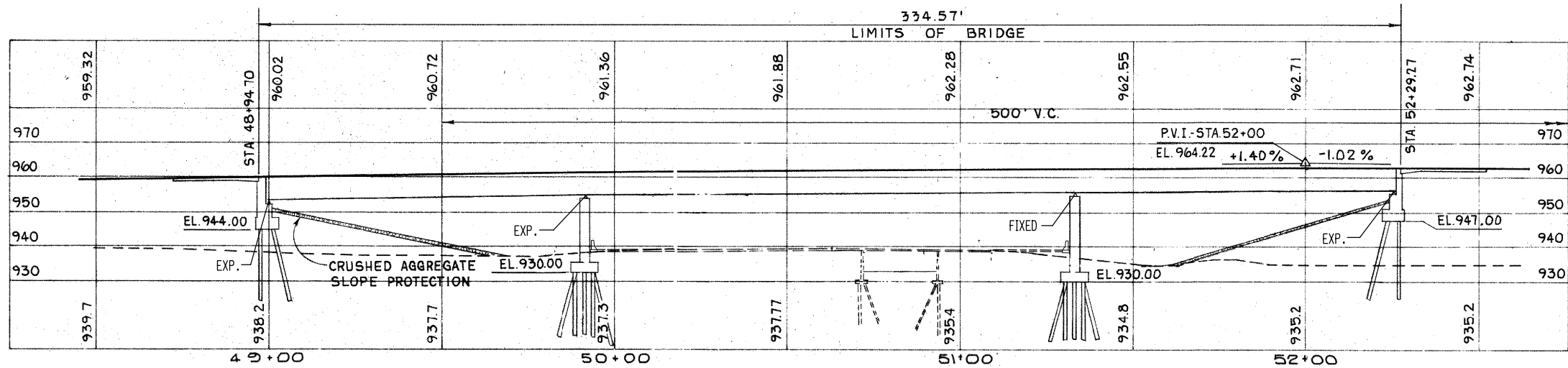
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63  
67





REF #	DIMENSIONS	LBS.
①	(6'-2")x4"x4"x $\frac{5}{8}$ "	50.57
②	(6'-6")x4"x4"x $\frac{5}{8}$ "	53.3
TOTALS		103.87



NOTES: 6

- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED  $\pm$  AND FOR REFERENCE ONLY.
- 2). BEARINGS NUMBERED 1-6 LEFT TO RIGHT LOOKING UPSTATION  
ON IR 675 (RAMP "V").
- 3). ABUTMENT BEARINGS ARE R-150 MODIFIED.
- 4). STANDARD DRAWING RB-1-55 DATED 2-2-59 (REVISED),  
SD-1-69 DATED 6-12-69.
- 5). SEE SHEET 5 FOR ADDITIONAL NOTES.
- 6). *STRUCTURE SHOWN FALLS WITHIN L/A RIGHT OF WAY FOR SR. 741/IR. 675*

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

## EXISTING STRUCTURE

TYPE: CONTINUOUS PLATE GIRDER WITH REINFORCED  
CONCRETE DECK AND SUBSTRUCTURE

SPANS: 92.0' - 141.5' - 92.0' ALONG @  
ROADWAY: 41'-8" F/F PARAPET

LOADING: HS20-44 CASE II & ALT MILITARY LOADING

SKEW: 56°-57'-40" L.F.

APPROACH SLABS: AS-1-72 (30' LONG)

ALIGNMENT: Dc = 3°00 TO RIGHT

CROWN: 0.071 FT./FT.

STRUCTURAL FILE NUMBER: 5710804

DATE BUILT: 7/1/1983

DISPOSITION: TO BE REPAIRED

## PROPOSED WORK

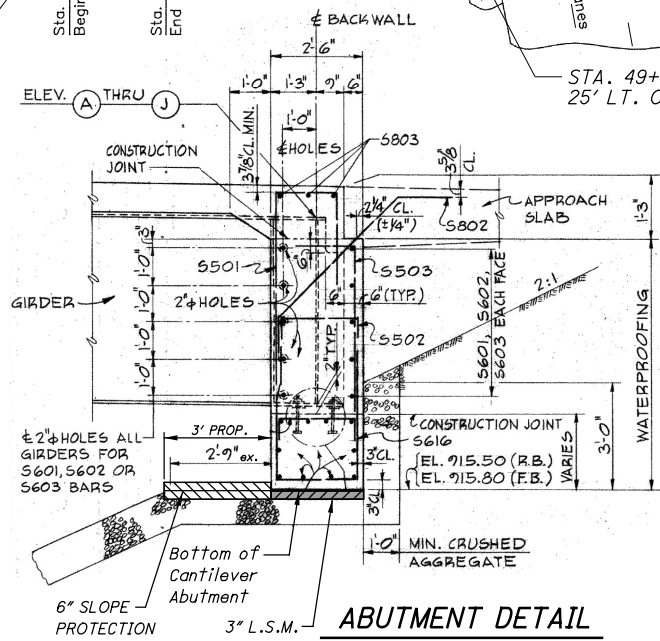
- 7). RESET BEARINGS #2, #3, #5 AT REAR ABUTMENT
- 8). RESET BEARINGS #2, #4, #5 AT FWD. ABUTMENT.
- 9). REPAIR END FRAME BETWEEN BEAMS #5 AND #6 AT FWD. ABUTMENT





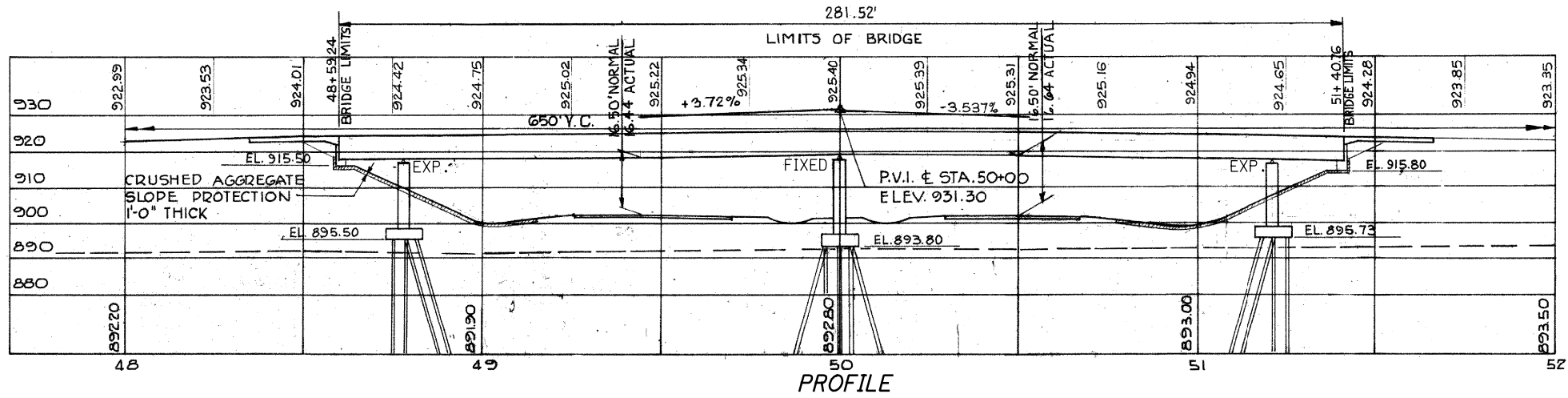
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REF #	ITEM	QUAN.
① FWD	613	1.62 CY.
① REAR	613	1.62 CY.
TOTALS (ROUNDED UP)		4 CY.
② FWD	601	23.3 SY.
② REAR	601	23.3 SY.
TOTALS (ROUNDED UP)		47 SY.



SLOPE PROTECTION DETAILS & NOTES

- ① = Item 613- Low Strength Mortar, 3" under both existing Cantilever Abutments. (3" x 2'-6" x 70')x2 each.
- ② = Item 601-Crushed Aggregate Slope Protection: Place crushed aggregate in 3' width in front of both abutments so that the aggregate extends a minimum 3" above the bottom of the abutment (Avg. Thickness = 6"),(Avg. Length 70').



- NOTES:
- DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
  - BEARINGS ARE NUMBERED 1-9 LEFT TO RIGHT LOOKING UPSTATION ON YANKEE ST.
  - PIER BEARINGS ARE R-300.
  - STANDARD DRAWING RB-1-55 DATED 2-2-59 (REVISED).
  - SEE SHEET 5 FOR ADDITIONAL NOTES.
  - EXISTING STRUCTURE FALLS WITHIN THE EXISTING LA RIGHT OF WAY FOR THE INTERCHANGE, AND OUTSIDE THIS VIEW AREA.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

**EXISTING STRUCTURE**

TYPE: CONTINUOUS STEEL BEAM WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: 18.0' - 122.0' - 122.0' - 18.0'

ROADWAY: 70'-0" F/F PARAPET

LOADING: HS 20-44 CASE II & THE ALT. MILITARY LOADING (ORIG.)

SKEW: 10°-15'-00" RT. FWD.

APPROACH SLABS: AS-1-72 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 3/16"/FT

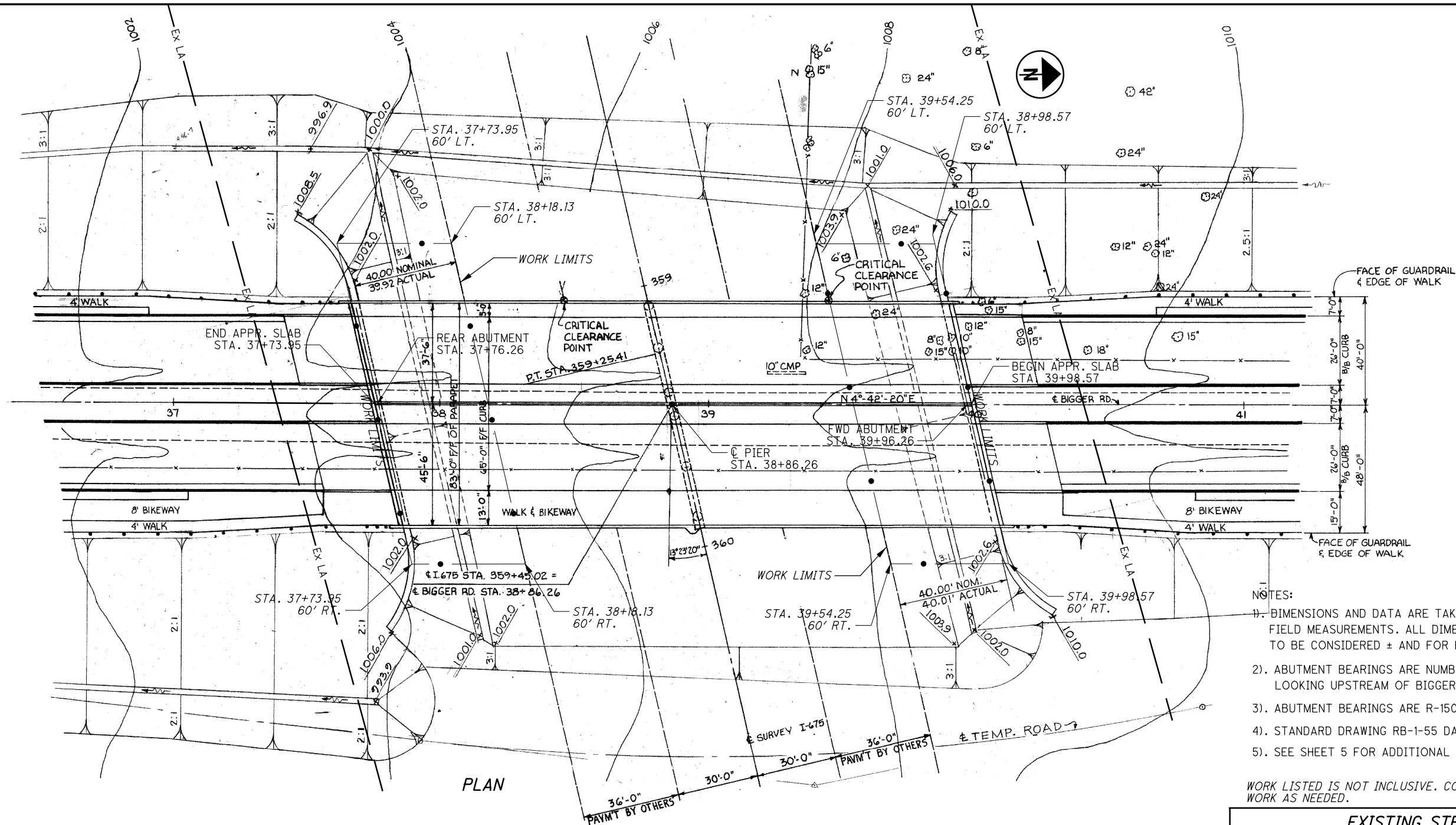
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DATE BUILT: 7/1/1983

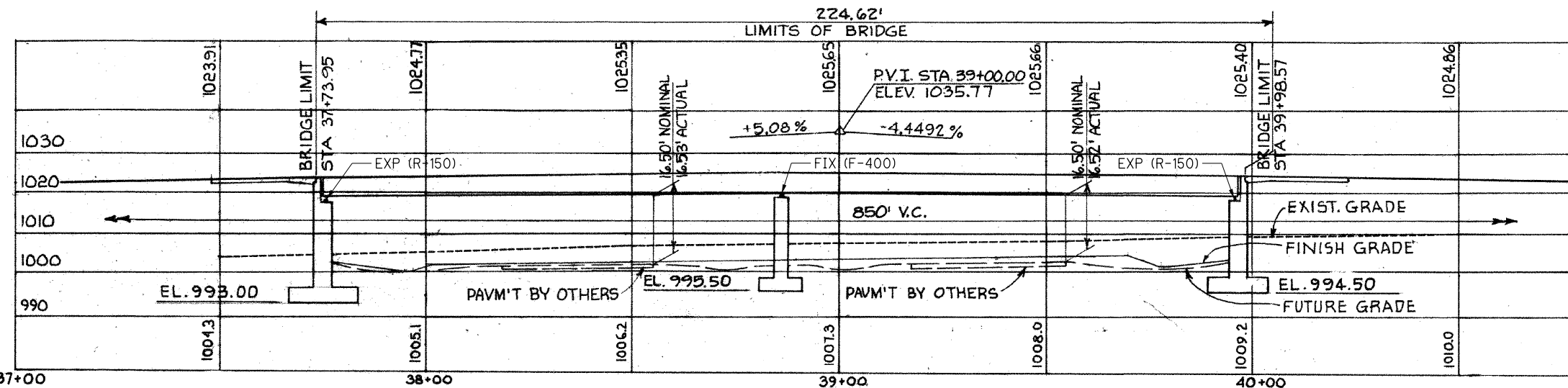
DISPOSITION: TO BE REPAIRED

- RESET BEARINGS #1 & #9 AT REAR PIER.
- REPAIR SLOPE PROTECTION AT REAR AND FWD ABUTMENTS.

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PLAN



PROFILE

- NOTES:
- 1). DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENTS. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED ± AND FOR REFERENCE ONLY.
  - 2). ABUTMENT BEARINGS ARE NUMBERED 1-11 LEFT TO RIGHT LOOKING UPSTREAM OF BIGGER RD.
  - 3). ABUTMENT BEARINGS ARE R-150.
  - 4). STANDARD DRAWING RB-1-55 DATED 2-2-59 (REVISED).
  - 5). SEE SHEET 5 FOR ADDITIONAL NOTES.

WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

### EXISTING STRUCTURE

TYPE: A588, (MINIMUM YIELD STRENGTH OF 50 KSI AND ALLOWABLE JACKING STRESS OF 25 KSI) PAINTED CONTINUOUS STEEL PLATE GIRDERS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: 110' - 110'

ROADWAY: 83'-0" F/F PARAPET

LOADING: HS 20-44 (CASE II) AND ALTERNATE MILITARY LOADING  
SKEW: 13°23'20" RT. FWD.

APPROACH SLABS: AS-1-81 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 1/4"/FT

STRUCTURAL FILE NUMBER: 5711436

DATE BUILT: 1985

DISPOSITION: TO BE REPAIRED

### PROPOSED WORK

- 1). RESET BEARINGS #1-#3, #6, #7, #11 REAR ABUTMENT
- 2). RESET BEARINGS #1-#5, #8 FWD. ABUTMENT

DESIGN AGENCY  
ODOT - DISTRICT 7  
ENGINEERING

DATE  
10/3/19  
REVIEWED  
MRB  
STRUCTURE FILE NUMBER  
5711436

DRAWN  
REB  
CHECKED  
DGH  
DESIGNED  
REB

MONTGOMERY COUNTY  
STA. 37+73.95  
STA. 39+98.57

SITE PLAN  
BRIDGE NO. MOT-675-0647  
BIGGER ROAD OVER I.R. 675

D07 BH FY20(B)  
PID No. 105417

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